

Specific Plan

North Chico

March 28, 1995

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**NORTH CHICO
SPECIFIC PLAN**

**PREPARED FOR:
THE BUTTE COUNTY BOARD OF SUPERVISORS**

**PREPARED BY:HERITAGE PARTNERS
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**APPROVED BY THE BOARD OF SUPERVISORS
MARCH 28, 1995**

1. INTRODUCTION

County Service Area 87

In 1983, as a result of drainage studies conducted in the north Chico area, County Service Area 87 (CSA 87) was formed to provide funding for maintenance of drainageways and drainage improvements in what now generally constitutes the North Chico Specific Plan area. However, no drainage improvements were authorized nor funded, and maintenance has consisted of occasional clearing of vegetation from creek channels.

Over the last several years development pressures have increased in the north Chico area as population in the Chico Urban Area has continued to increase. In order to allow development to proceed in a responsible manner, areawide issues related to drainage, circulation, and provision of public services must be addressed.

The Planning Process

The Butte County Board of Supervisors initiated the preparation of the North Chico Specific Plan to comprehensively respond to development proposals and incorporate them into a concept of land use for the area, while evaluating and providing for areawide solutions to drainage, circulation, and provision of public services. This planning and environmental review process has been conducted through expansion of the authority of CSA 87. At the request of the Board of Supervisors, the Local Agency Formation Commission authorized the addition of areawide land use planning to the previous county service area scope of drainage maintenance services.

Subsequent assessment of properties within CSA 87 provided the necessary funding. To provide a degree of balance and equity, a "two-tier" assessment was levied. A nominal rate for existing homes and small parcels provided a fraction of the total funds. A second, per-acre assessment for larger, potentially developable parcels provided the bulk of the funds in rough proportion to expected landowner benefit.

Numerous meetings with property owners and residents of the Plan area have been held during which presentation and discussion of planning goals and ideas occurred. Concerns regarding the preservation of existing neighborhood characteristics, the cost of potential road and drainage improvements, and the desire of owners of vacant land to beneficially utilize their property were extensively discussed.

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In addition to community meetings, presentations and workshop meetings were conducted with many local agency representatives and interest groups, including:

- the Butte County Airport Land Use Commission
- the Butte County Local Agency Formation Commission
- the Butte County Planning Commission
- the City of Chico General Plan Task Force
- the Greater Chico Chamber of Commerce Airport Committee
- the Greater Chico Chamber of Commerce Economic Development Committee
- the Intergovernmental / Chico Issues Committee

Economic Development

In addition to the resolution of the planning issues cited herein, the process has examined the preparation of lands in the vicinity of the Chico Municipal Airport for job-creation and economically beneficial uses. The adjacency of the Chico Municipal Airport provides a unique opportunity to locate industrial, commercial, and office uses which would utilize and heighten the importance of the Chico Municipal Airport as both a carrier of freight and passengers. The addition of industrial land adjacent to the airport and improved vehicular access will help to insure the long term viability of the airport.

Purpose of a Specific Plan

A specific plan is fundamentally a tool for the "systematic implementation" of the general plan, typically within a defined area. Because the general plan must address policy issues on a broad scale and throughout the agency's jurisdiction, it cannot easily deal with the needs of a smaller area. Although the specific plan must be consistent with the general plan, a specific plan can address infrastructure, land use, and financial issues in a more appropriately focused and detailed manner.

A specific plan is prepared, adopted and amended in the same manner as the general plan, except that it may be adopted by resolution or ordinance and it may be amended as often as deemed necessary by the local agency. Once a specific plan is adopted, zoning ordinances, subdivisions, public works projects, development agreements, and land projects must be consistent with the specific plan.

The purpose of the North Chico Specific Plan (NCSP) is to provide a comprehensive description of all land uses provided for in the Plan area and to specify concise policies and regulations which implement the broader goals and policies of the Butte County General Plan. The NCSP is a specific plan developed within the framework established by California Government Code Section 65450 et seq.

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All the requirements of the Government Code pertaining to the preparation, approval, and implementation of specific plans are addressed in this document including text and diagrams specifying the following:

- The distribution, location, and extent of the uses of land, including open space, within the area covered by the Plan;
- the proposed distribution of major components of transportation, sewage, water, drainage, solid waste disposal, energy, and other essential facilities proposed to be located within the area covered by the Plan and needed to support the land uses described in the Plan;
- standards and criteria by which development will proceed, and standards for the conservation, development, and utilization of natural resources, where applicable;
- a program of implementation measures including regulations, programs, public works projects, and financing measures necessary to carry out the above; and,
- a statement of the relationship of the North Chico Specific Plan to the Butte County General Plan.

This Plan is intended to focus and assist the development planning and review process by providing a direct and comprehensive correlation between land use and public facilities and services necessary for support of that development. The NCSP includes an infrastructure and facilities element, and an implementation and financing element which together identify all necessary services and facilities and the means of financing those improvements.

In a similar manner, the environmental review process is simplified and streamlined by allowing an overall evaluation of impacts associated with development of the entire Plan area. In this manner, individual applications for development will be able to utilize the NCSP certified EIR in their environmental review, thus reducing costs and processing time. Some properties may require focused studies for the delineation of wetland area, biological studies for flora and fauna specific to their proposed development. It is anticipated that most development projects, if found to be consistent with the NCSP and the certified EIR, will be able to utilize a negative declaration for their environmental determination.

The NCSP is intended to serve as a broad reaching site planning tool to allow for creative and imaginative large scale planning of large scale, mixed-use development. The NCSP also allows the County to tailor zoning or development regulations to fit the dictates of the specific plan.

2. SPECIFIC PLAN OVERVIEW

Project Location and Setting

The North Chico Specific Plan (NCSP) area is located north of the City of Chico as shown on Figure 2-1. The 3,590-acre area is bounded by Sycamore Creek on the south, State Route 99 on the west, Rock Creek on the north and Chico Municipal Airport on the east. Two small portions of the study area, consisting of approximately 180 acres, are located south of Sycamore Creek, see Figure 2-1.

The area primarily contains open space lands, but a pattern of rural and semi-rural residential development has resulted due to development of several subdivisions located along Keefer Road, east of State Route 99 (SR99), and off of Hicks Lane in the vicinity of Sycamore Creek. The existing land use in the project area is a mix of agriculture, open space, and single-family residential uses with an estimated 640 dwelling units and 1,450 residents. Lot size are variable, ranging from 1 to 5 ± acres in the area adjacent to Keefer Road to smaller, more suburban-sized lots (0.5 ± acres) in the subdivisions east of SR99.

The remainder of the project area is undeveloped, consisting of agricultural uses, mostly in the western half of the study area, and open space/grazing lands in the eastern portion of the study area. A former quarry is located in the central portion of the area, north of Mud Creek and east of Garner Lane. A small area off of State Route 99 and Garner Lane contains heavy commercial and light industrial uses.

The NCSP area is relatively flat and bisected by four major creeks and drain-ages: Sycamore Creek on the south, Mud Creek and Keefer Slough in the central portion of the area and Rock Creek on the north. Natural vegetation is sparse.

Site Constraints and Planning Issues

Prior to developing the draft North Chico Specific Plan, numerous community meetings, data collection, field surveying, agency contacts, and an agency workshop were conducted and a description of existing conditions and constraints of the plan area was prepared. This information was used in developing the Specific Plan land use concept and elements. The specific site constraints and planning issues that were considered in development of the Specific Plan are summarized in Table 2-1.

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Summary of Specific Plan Elements

The NCSP contains six elements:

- land use,
- circulation,
- open space/recreation/resource management,
- provision of public services,
- funding, and
- implementation measures.

Each element explains the development concepts. In general, the land uses proposed in the NCSP are primarily residential with a mix of commercial, office, heavy and light industrial, public (elementary school, park, and fire station), and open space. Intensive development is concentrated south of Mud Creek. North of Mud Creek, large-lot residential uses are left unchanged in order to maintain and foster compatibility with existing development and to maintain the existing rural character of the area.

**TABLE 2-1
SUMMARY OF CONSTRAINTS & PLANNING ISSUES
CONSIDERED BY THE NORTH CHICO SPECIFIC PLAN**

Land Use:	Existing residents desire to maintain rural character; consistency with General Plan; and potential conflicts between land use (i.e., areas adjacent to airport, agricultural uses, or commercial/ industrial uses).
Hydrology:	Existing flood-prone areas primarily along Keefer Slough and Sycamore Creek; inadequate flood capacity along Keefer Slough; inadequate flood capacity through local bridges and culverts; and U.S. Army Corps of Engineers permit requirements for discharging additional flows into Sycamore Creek.
Biology:	Significant wetland acreage exists primarily in southeastermost portion of the plan area; riparian vegetation and habitat occurs along western end of Keefer Slough; and presence of special status species.
Circulation:	Intersection of Keefer road and State Route 99 has existing traffic problems; the geometrics of Eaton Road, State Route 99, and Hicks Lane intersection will have traffic problems with additional growth; lack of east/west connector through the plan area; and potential prohibition of new signalized intersections on State Route 99.
Noise:	Proximity to the Chico Municipal Airport and State Route 99.
Public Services:	No sewer facilities exist in the Plan area; soil conditions limit use of septic systems; sewer treatment plant approaching capacity; City policy requiring that any area receiving City sewer services be annexed into the City; insufficient capacity in Chico Unified School District to accommodate Plan area growth; regional landfill approaching capacity; park standard for provision of park and recreational facilities; proposed park areas in Plan area; existing understaffed Sheriff's Department; and need for additional police and fire staff, equipment, and facilities to serve Plan area growth.

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Land Use

The majority of the Plan area is planned for low density suburban residential development (1 to 3 acre lots) north of Mud Creek. Low and medium density residential development (4 to 8 units per acre for single-family) and a high density residential area (7.1 to 18 units per acre) are proposed south of Mud Creek. A total of approximately 2,803 new dwelling units are contemplated under the proposed Plan.

The Village Core is the heart of the specific plan. Commercial, office, public (elementary school site and park site), and multi-family residential uses would be located in the Village Core. It is centrally located within the Plan area south of Mud Creek and north of Sycamore Creek. The Village Core will provide area residents with a central commercial shopping and professional services area in close proximity to other community services and amenities, such as an elementary school and park. Industrial development is proposed east and southeast of the Village Core, along the easternmost boundary of the plan area, adjacent to the Chico Municipal Airport.

Between 412 and 580 acres are designated for open space and parks, a portion of which are located along area creeks and drainages. Open space corridors are located throughout the Plan area and serve as recreational corridors, drainage courses, resources, and provide buffers between land uses.

Circulation

The NCSP provides for a comprehensive circulation system that includes streets, paths, and trails designed to facilitate safe and efficient movement within and through the Plan area for improved automobile, pedestrian, bicycle, and equestrian travel.

Access to the Village Core and industrial development would be provided primarily by widening and realigning Hicks Lane northerly from Eaton Road through the Plan area, and constructing a new arterial road extending from the village core westerly to State Route 99. This route would become a new east-west route through the plan area, with construction of expanded or new bridges over Sycamore and Mud Creeks, respectively. It would provide for improved internal circulation, as well as alternative access routes into the plan area. Pedestrian paths and trails provide a non-vehicular travel alternative for the convenience of walkers, joggers, bicyclists, and equestrians.

Parks, Open Space and Resource Management

The NCSP provides for a wide variety of recreational and open space uses within the Plan area.

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The Plan area devoted to either natural or improved open space, including, parks, greenbelts and trail corridors, and natural open spaces areas such as wetlands and drainage corridors ranges from 412 to 580 acres (exact acreage will depend on site specific biotic surveys; see Chapter 5). The active use areas will primarily consist of parkland. Passive areas will include wetland areas and portions of the natural waterway corridors. They will be required to be maintained and/or enhanced.

Open space plays a key role in the design and function of the Plan area. While providing for wildlife habitat and passive recreation, it also serves to separate and define the neighborhoods that make up the Plan area. Further, the preservation and maintenance of large areas of natural open space enhance the overall aesthetic and visual character of the Plan area and the community. Open space lands throughout the Plan area are intended to protect natural and cultural resources (wetlands, riparian vegetation, major drainages, and sensitive cultural resources), and to maintain the rural character of the area.

Public Facilities & Services

Water, sewer, drainage, and other facilities and public services are needed to serve the Plan area population. Domestic water supply will be provided by California Water Service Company. A minimum of 7 new wells and water distribution mains will be necessary to serve the Plan area.

Plan area development north of Mud Creek will rely on septic systems, whereas public sewers will be provided for new development south of Mud Creek. The NCSP presents two alternatives for provision of sanitary sewer service. The first alternative considers connection to the City of Chico sewer system via connection to a new, as yet unconstructed, trunk line along Eaton Road or connection (on an interim basis) to existing lines with construction of a new force main sewer line and pump station.

Under the second alternative, a package treatment plant would be constructed at an unidentified location west of State Route 99. Treated effluent from the package plant could be discharged into Mud Creek, used for agricultural irrigation, and/or for wetland enhancement/creation programs.

Storm drainage management consists of a combination of facilities including channel improvements to existing creeks, detention basin(s), and natural drainage swales. Stormwater runoff from proposed Plan area development would be directed to Keefer Slough, Mud Creek, and Sycamore Creek through a system of open channels (incorporating roadside ditches and greenbelt drainage swales). This open channel system will be combined with a closed conduit drainage system and on-site detention areas in the southern and southeastern portions of the Plan area.

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Development Regulations and Standards

The regulations and design standards to guide future development are detailed here. Development standards include the County zoning ordinance requirements, as well as general development standards specific to the NCSP area. Standards are also presented for road construction, landscaping, and design.

Implementation

Capital improvements that will be needed for buildout of the Plan area and financing strategies needed to fund those improvements are contained here. Other necessary implementation actions for also include processing of subsequent development proposals.

Specific Plan Goals and Policies

The overall goal of the NCSP is to create a detailed and comprehensive plan for the area which implement the goals and policies of the Butte County General Plan. The NCSP is a more detailed level of planning with goals and policies tailored to the NCSP area. The goals of the Plan are summarized in Table 2-2. NCSP policies are summarized in Table 2-3 and are intended to achieve the goals of the Specific Plan and to provide direction to decision makers in the implementation and administration of this Plan.

Relationship of Specific Plan to Butte County General Plan

The NCSP planning process included a general plan amendment to bring the specific plan into consistency with the Butte County General Plan. While the current general plan is being updated it is anticipated that the update will incorporate the NCSP, as adopted by the Board into the general plan update.

A detailed review of the NCSP consistency with all existing General Plan goals, objectives, and policies is provided in Appendix A.

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**TABLE 2-2
NORTH CHICO SPECIFIC PLAN GOALS**

- ① Create a functional and attractive residential community, complimented by cultural amenities and all public facilities and services necessary to support the population which will result from Plan development, with development of a variety of housing types to accommodate a broad range of household needs.
- ② Create integrated open space, parks and recreational amenities which will result in improved quality of life for residents of both the Plan area and greater Chico area.
3. Plan areas for commercial goods and services, and employment opportunities which will meet the needs of area residents and reduce the need for daily travel outside the Plan area. Traffic on all streets within the plan area should not exceed a Level of Service (LOS) "C". In no case shall the LOS exceed "D".
4. Develop a circulation system which provides for the efficient and uncongested movement of vehicles, pedestrians, and bicyclists with minimum intrusion upon the rural and residential character of the area.
5. Increase the mobility of residents through development of an adequate and balanced transportation system that includes automotive and non-vehicular transportation considerations.
6. Develop a community complimented by compatible and harmonious architectural and landscape design.
7. Establish a balanced, pedestrian-oriented Village Core which supports a variety of uses.
- ⑧ Provide public and community services which both adequately serve the community and are cost-effective.

**TABLE 2-3
SPECIFIC PLAN POLICIES**

General Policies

- ① Create a functional and attractive residential community, with development of a variety of housing types to accommodate a broad range of household needs complimented by cultural amenities and all public facilities and services necessary to support the population which will result from Plan development.
- ② Create integrated open space, parks and recreational amenities which will result in improved quality of life for residents of both the Plan area and greater Chico area.
3. Provide commercial goods and services, and employment opportunities which will meet the needs of area residents and reduce the need for daily travel outside the Plan area.
4. Develop a circulation system which provides for the efficient and uncongested movement of vehicles, pedestrians, and bicyclists with minimum intrusion upon the rural and residential character of the area.
5. Increase the mobility of the residents through development of a transportation system which includes automotive and non-vehicular transportation.
6. Develop a community of compatible and harmonious architectural and landscape design.
7. Establish a balanced, pedestrian-oriented Village Core which supports a variety of uses and provide a focal point for the area.
- ⑧ Provide public and community services which both adequately serve the community and are cost effective.
9. Preserve the semi-rural lifestyle of the Plan area, while providing for the future housing needs.
10. Protect the long-term operations of the Chico Municipal Airport by providing compatible land uses, adequate setbacks, aviation easements, signing, and other measures.

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Agricultural Policies

All existing orchards shall be permitted to continue, without interference or interruption by development.

Residential Policies

1. Provide buffers between residential uses and industrial uses and/or arterial thoroughfare streets by using landscaped corridors, berms, greenbelts, and/or setbacks.
2. Preserve and enhance the existing land use patterns and densities north of Mud Creek.
3. Develop a pedestrian-oriented Village Core which provides shopping, social and recreational facilities for the Plan area.
4. Locate multi-family residential uses in and adjacent to the Village Core to maximize the benefits of community amenities and to minimize travel within the development.
5. Provide for a mix of land uses (residential, retail and public) in the Village Core.

Commercial/Industrial Policies

1. Locate and orient commercial buildings to minimize negative impacts on adjoining residences. Outdoor activity areas and noise-generating equipment should be located away from residential areas.
2. Incorporate outdoor public gathering areas, including fountains, arbors, seats, public art, or other elements, into commercial sites to facilitate social interaction by Plan area residents and employees.
3. Identify opportunities to integrate economically beneficial activities such as the designation or improvement of industrial land or the removal of constraints to the development of existing industrially-zoned land adjacent to the Plan area and westerly of the airport.

Circulation Policies

1. Limit access from residences and businesses along major arterials to a general spacing of 1 access point for each 300 feet. Utilize reciprocal access agreements, shared driveways, easements and other methods. In no case is it the intent of this policy to deny vehicular access to any legal lot.
2. Design local streets within residential neighborhoods to discourage automobile and truck through-traffic by use of cul-de-sac streets or other design techniques.
3. In order to encourage use of the proposed paths/trail system, design street patterns which utilize single loaded streets and other street and lot design techniques along proposed paths/trails.
4. Where cul-de-sac streets are proposed, utilize designs which will provide pedestrian/bicycle (and equestrian where appropriate) access. This access should lead to or connect to the overall planned path/trail system. Key lots (lots located at the end of cul-de-sac streets) should be minimized.
5. In order to encourage public transit usage, the arterial street design shall provide for bus turnouts and for the location of bus shelters.

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6. All path and trails designated within properties which are proposed for subdivision shall be required to be designed and improved by the developer of the subdivision in accordance with the requirements of this Plan and good planning and engineering practice. Maintenance of these areas will be the responsibility of CSA 87.
7. Encourage non-vehicular access through the Plan area and to the commercial and industrial areas by requiring the construction of sidewalks, pedestrian/bicycle paths and bicycle parking facilities.
8. Enhanced street entries shall be required at the following location:
 - a. The new arterial/Highway 99.
 - b. The north and south entry to the industrial area.
 - c. The new Hicks Road/Eaton Road.
 - d. The new arterial near Mud Creek.
 - e. The new arterial near Sycamore Creek.

The entry shall consist of landscaping and hardscape. Rock or block walls shall be installed behind the sidewalk. The name of each land use area shall be designed into the industrial Park, and the like. Shrubbery and at least four specimen size trees shall be used to further enhance the entry. Each entry area shall also have a raised landscaped median with a left turn pocket. The design of the street entries shall be approved by the Directors of Development Services and Public Works.

9. A detailed alignment study for Old Hicks Road/New Hicks Road and Eaton Road shall be prepared by or under the direction of Butte County. The study shall indicate the precise engineering of the (right-of-way) alignment, the removal or relocation of structures, the installation of curb, gutter, sidewalk, and other infrastructure necessary to accomplish the realignment.
10. Noise attenuation along existing and proposed arterials shall be required to protect residential development proposed to be located adjacent to the proposed arterials. Noise attenuation measures shall be required to reduce interior noise levels to 45 dB for proposed residential development adjacent to existing or planned arterials and/or when adjacent to Highway 99.
11. No new street alignments shall exclude safe and convenient access to properties. County staff shall pursue negotiations with Caltrans for appropriate traffic control, including, but not limited to, traffic signals and street alignments along Highway 99, primarily at the intersections of Keefer Road and the new arterial.

Parks and Open Space Policies

1. Provide a network of linear open space along identified drainage and creek areas and connecting paths/trails to the Village Core and industrial area.
2. Provide parks and recreational facilities located in order to adequately serve the existing and future residents of the Plan area. Incorporate, as appropriate, wetland and riparian habitat protection, and nature interpretive facilities.
3. Provide a process where the Plan area residents and owners can participate in the design of the parks and the recreational facilities.
4. Locate the neighborhood park sites adjacent to the elementary school sites and encourage CARD and the Chico Unified School District to enter into a joint-use agreement.
5. Require that all park and open space areas maintenance are funded through the amendment of the existing CSA or other method acceptable to LAFCo, Butte County, and the responsible maintenance group or agency.

Environmental Protection Policies

1. Minimize impacts to archaeological, biological, riparian and other natural resources through avoidance.
2. Utilize avoidance as the main form of mitigation of impacts on identified wetlands. Where avoidance is not possible due to the need to provide critical feature of the NCSP, a no net loss concept will be used. This may require the acquisition of unencumbered wetlands, payment into a wetlands mitigation fund, or other appropriate mitigation in effect at the time of development.
3. If found to be compatible with the protection of wetland resource areas, reserve locations in major wetland preservation areas for possible nature interpretation areas, such as viewing or lookout areas and interpretive walking trails.

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Water Policies

1. Plan for the installation of public water facilities in the Plan area south of Mud Creek, while continuing to allow for the use of well water for those developments north of Mud Creek.
2. In the area north of Mud Creek, require the use of community water system may be required parcels. The community water system shall be established in accordance with the requirements of state law.

Wastewater Disposal Policies

1. Prior to any development in the Plan area south of Mud Creek, a sewer alternative must be selected. The selected alternative shall meet the requirements of state law and Butte County.
2. In the area north of Mud Creek, community septic systems may be required parcels. Operation and maintenance of these systems shall meet the requirements of state law and standard engineering practice.
3. In the area north of Mud Creek, developments shall meet the requirements of Department of Environmental Health relative to the installation of septic systems.

Drainage Policies

1. Utilize, to the maximum extent possible, natural, or natural-appearing, stream or drainage courses for storm drainage.
2. Design stormwater facilities, including detention basins, to ensure public safety, to be visually unobtrusive and to provide, where feasible, recreational uses.
3. Development within a designated floodplain shall be required to meet County and FEMA regulations. No development shall be permitted in the floodway.
4. Utilize pollution control traps or sediment basins for storm drainage systems designed in accordance with County standards and/or requirements.
5. All drainage areas shall be maintained through amendment to the CSA, or other means acceptable to LAFCo, Butte County, and the maintaining group or agency.

Schools Policies

1. In consultation with the Chico Unified School District, plan for educational facilities in the Plan area.
2. Locate educational facilities in the Plan area adjacent to neighborhood parks in order to facilitate joint use recreation facilities.
3. Encourage CUSD and CARD to work cooperatively to provide joint use recreation facilities.
4. Impacts to school facilities within the Chico Unified School District (CUSD) shall be fully mitigated through the payment of mitigation fees in the amounts justified by CUSD prepared nexus studies, including adjustments, and adopted by the District, or through the implementation of other equivalent measures acceptable to CUSD, including those measures specified in the CUSD Board of Education Resolution No. 486-92.

The County recognizes that the new residential growth contemplated by this Specific Plan will result in significant increases in the student population of the Chico Unified School District ("CUSD"). The County also recognizes that funding limitations have severely hampered CUSD's ability to accommodate students generated by new residential development. Accordingly, the County, as a condition of approval of this Specific Plan, has required that new residential development fully mitigate its impacts to school facilities. But for this full mitigation requirement, the County would not have approved this Specific Plan.

Fire

1. Provide for the location of a future fire station and work with the appropriate fire service agencies to determine the nature and level of fire services required to serve the existing and future residents.
2. Amend the existing CSA, or provide another funding mechanism, to provide funds for continued fire services.

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Financing Plan Policies

Financing Plan Policies have been established to ensure that the development in the NCSP area: (a) pays the full costs of the infrastructure needed to serve the area; (b) funds the costs of mitigating adverse impacts on existing Butte County and City of Chico infrastructure, where affected; (c) provides for reimbursements from other development areas for costs that the NCSP was required to advance (if any); and (d) provides a fair allocation of costs between land uses.

The financing policies are set forth as follows:

Policy 1: Fund the full costs of on- and off-site public infrastructure and public services required to support development in the Plan area from revenues generated by development within the Plan area.

Policy 2: Use pay-as-you-go financing to the extent possible. Use debt financing only when essential to provide facilities necessary to permit development or to maintain service standards.

Policy 3: Allocate the backbone infra-structure costs to property within the Plan area based on the general principles of benefit received, with consideration to the financial feasibility of the proposed land use.

Policy 4: Require developers who proceed ahead of the infrastructure financing sequence to pay the costs of extending the backbone infrastructure to their project subject to future reimbursement.

Policy 5: Landowners who do not wish to develop will not be required to participate in debt financing that would result in a financial obligation on their property.

3. LAND USE ELEMENT

Purpose

The purpose and intent of this element is to detail the land use concept, set forth the goals and policies which will guide the development, preservation and continued viability of the NCSP area. The goals and policies of this element will provide the structure or framework for the remaining elements of the NCSP.

Existing Land Use

The North Chico Specific Plan (NCSP) consists of 3,590 acres of land generally bounded by Sycamore Creek on the south, State Route 99 on the west, Rock Creek on the north, and Chico Municipal Airport and Airport Clear Zone on the east. A small portion of the Plan area, approximately 170 acres, is located north of and adjacent to Eaton Road and south of Sycamore Creek.

Existing land uses are a mix of agriculture, open space, and single-family residential uses, currently with an estimated 640 dwelling units and 1,450 residents. The area contains predominately undeveloped lands, with some rural and semi-rural residential development evident along Keefer Road, Garner Lane and off of Hicks Lane in the vicinity of Sycamore Creek. Existing lot size varies from 1 to 5 ± acres in the area adjacent to Keefer Road to smaller, more suburban-sized lots (0.5 ± acres) east of State Route 99.

Most of the project area (2,100 ± acres) is undeveloped, consisting of agricultural uses in the western half of the area, and open space grazing lands in the eastern portion area. A former quarry is located in the central portion of the area, north of Mud Creek and east of Garner Lane. An area near State Route 99 and the Esplanade contains heavy commercial and light industrial land uses.

The Land Use Concept

The land use concept for the NCSP utilizes a "Village Core" concept in conjunction with utilizing the adjacency of the Chico Municipal Airport as an attractor for industry and commercial uses. The Plan takes advantage of the existing drainage courses designating them for greenbelt connectors, as well as for drainage. A new east/west and north/south arterial is proposed to provide safe and convenient access from SR99 and Eaton Road.

A key component in the NCSP is the "Village Core" encompassing approximately 285 acres southeast of Mud Creek, where the higher density housing, commercial, and industrial uses are concentrated.

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The commercial and industrial areas are the key incentive to providing mix of land use to assist in reducing automobile dependence and in encouraging pedestrian/bicycle travel. The Village Core will provide an integrated community containing housing, shops, work places, schools, parks, and civic facilities essential to the daily life of the area residents. The area of the Village Core is designed so that all activities are within easy walking distances to each other. It will also serve as a community focal point by providing an activity center and a place for formal and informal social and community interaction.

The physical design of the Village Core will create an aesthetic composition of buildings and open space that will encourage frequent use. Development regulations and guidelines contained in Chapter 7 of this document, will prevent the standard subdivision, industrial, and strip shopping center look.

Proposed Land uses in the NCSP area are primarily single family and multiple family residential with a mix of commercial, office, heavy and light industrial. The area will also contain a public elementary school with an adjacent neighborhood park, a fire station, and open space. Intensive development is concentrated south and east of Mud Creek. Northwest of Mud Creek, the existing planned large lot residential uses are to remain ensuring compatibility with existing development and maintaining the existing rural character of the area.

Over 500 acres are designated for open space/drainage. These corridors are located throughout the Plan area and serve as multipurpose trails, drainage corridors and buffers between land uses. The proposed Specific Plan map is presented on Figure 3-1; land uses are summarized on Table 3-1.

Residential Uses

The NCSP accommodates a broad spectrum of housing types and densities through five residential zones/land uses. There is a maximum potential for approximately 2,803 additional new dwelling units within the NCSP.

The majority of residential acreage will be single-family detached homes on one acre lots located northwest of Mud Creek. The remainder of residential uses will be a mix of more urban densities within the Village Core area and south of Sycamore Creek. Up to 1,741 dwelling units could be constructed in areas designated for 4 units per acre and 6 to 8 units per acre. In addition to the single-family units, multi-family housing is proposed on 17.1 acres within the Village Core, which could result in approximately 308 dwelling units.

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**TABLE 3-1
SUMMARY OF NORTH CHICO SPECIFIC PLAN DEVELOPMENT**

ZONING / LAND USE		EXISTING		NEW		BUILDOUT	
		ACRES	UNITS	ACRES	UNITS	ACRES	UNITS
SR-3	Suburban Residential - 3-acre minimum parcel size	114.2	26	109.8	35	224.0	61
SR-1	Suburban Residential - 1-acre minimum parcel size	1098.8	613	686.4	678	1785.2	1291
SR-1/OS	Suburban Residential - 1-acre min. parcel size (@ 30% yield)	—	—	138.2	41	138.2	41
R-1	Low Density Residential - Average 4 DU/AC	—	—	222.2	889	222.2	889
R-1/OS	Low Density Residential - Average 4 DU/AC (@ 50% yield)	—	—	11.7	23	11.7	23
R-2	Medium Density Residential - (Average 7 DU/AC)	—	—	118.4	829	118.4	829
R-3	High Density Residential - (Average 18 DU/AC)	—	—	17.1	308	17.1	308
SUBTOTAL RESIDENTIAL		1213.0	639	1303.8	2,803	2516.8	3,442
M-2	Heavy Industrial	—	—	—	—	—	—
M-2/OS	Heavy Industrial / Open Space	—	—	251.2	—	251.2	—
M-1	Limited Industrial	35.5	—	124.8	—	160.3	—
M-1/OS	Limited Industrial / Open Space	—	—	115.8	—	115.8	—
C-2	General Commercial	37.4	—	12.2	—	49.6	—
C-1	Light Commercial	—	—	12.4	—	12.4	—
BP	Business & Professional	—	—	26.0	—	26.0	—
PQ	Public, Quasi-Public	—	—	65.8	—	65.8	—
OS	Greenbelt / Open Space	187.1	—	204.1	—	391.2	—
SUBTOTAL NON-RESIDENTIAL		260.0	—	812.3	—	1072.3	—
TOTAL		1483.0	639	2116.1	2,803	3589.1	3,442

North Chico Specific Plan LAND USE MAP

Figure 3-1

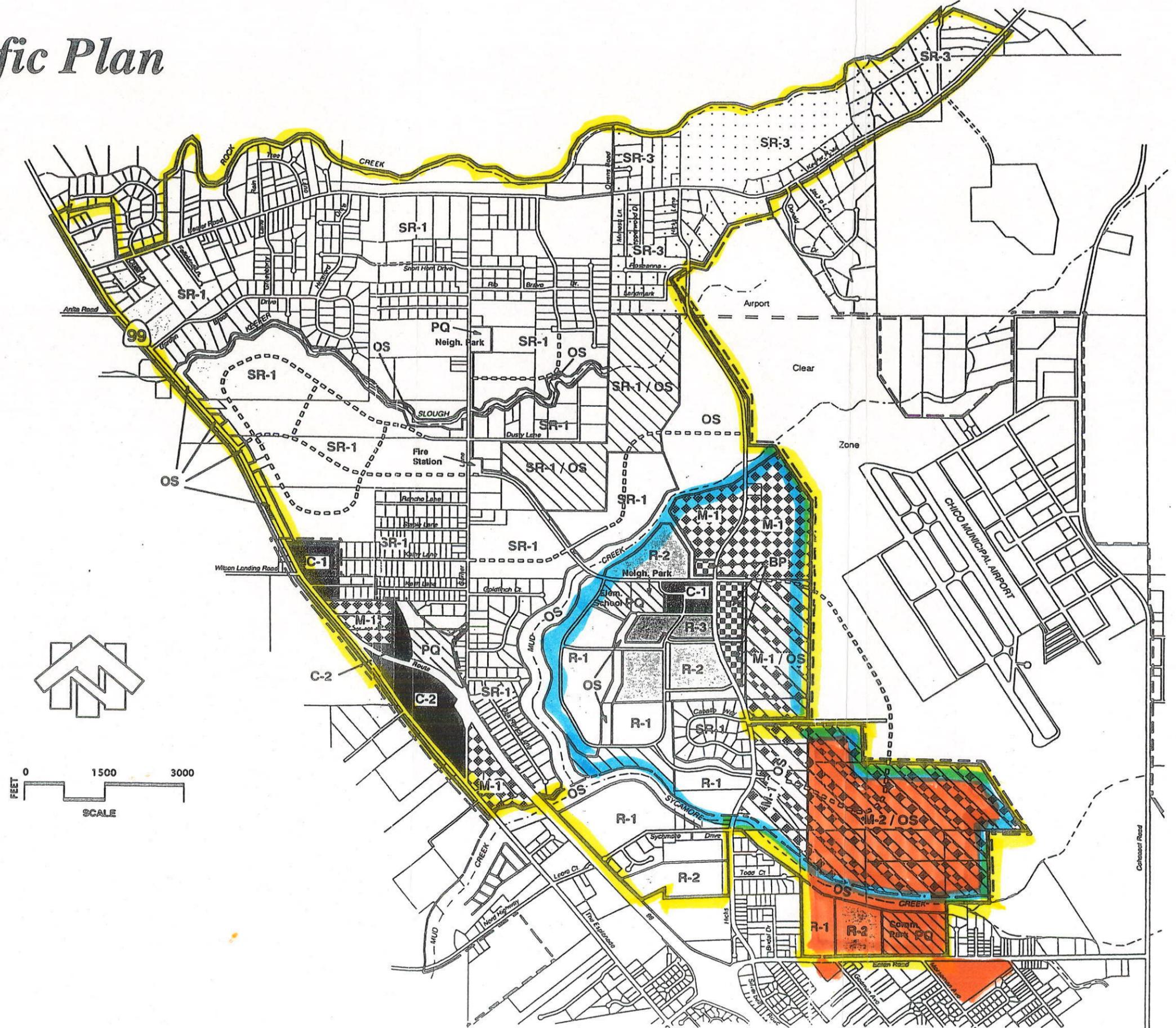
LEGEND

-  SPECIFIC PLAN AREA BOUNDARY
-  SPHERE OF INFLUENCE (CITY OF CHICO)
-  STREET WITH FLEXIBLE ALIGNMENT
-  STREET WITH FIXED ALIGNMENT

LAND USE

- | | | |
|---|------|---------------------------|
|  | SR-3 | SUBURBAN RESI. 3 AC. MIN. |
|  | SR-1 | SUBURBAN RESI. 1 AC. MIN. |
|  | R-1 | LOW DENSITY RESIDENTIAL |
|  | R-2 | MED. DENSITY RESIDENTIAL |
|  | R-3 | HIGH DENSITY RESIDENTIAL |
|  | M-2 | HEAVY INDUSTRIAL |
|  | M-1 | LIGHT INDUSTRIAL |
|  | C-2 | GENERAL COMMERCIAL |
|  | C-1 | LIMITED COMMERCIAL |
|  | BP | BUSINESS & PROFESSIONAL |
|  | PQ | PUBLIC / QUASI-PUBLIC |
|  | OS | GREENBELT / OPEN SPACE |

THE PRECISE BOUNDARY OF THE "GREENBELT / OPEN SPACE" LAND USE DESIGNATION SHALL BE DETERMINED AT THE TIME OF DEVELOPMENT. CROSS-HATCH INDICATES LANDS THAT MAY CONTAIN BIOLOGICAL RESOURCES OR SENSITIVE HABITAT AND REQUIRE SITE-SPECIFIC PLANNING AND REVIEW PRIOR TO DEVELOPMENT. ALTERNATING COLOR INDICATES UNDERLYING LAND USE CATEGORY.



NORTH CHICO SPECIFIC PLAN

Commercial and Industrial Uses

The NCSP designates approximately 12.4 acres of limited commercial uses, 12.2 acres of general commercial uses, 26.0 acres of offices, 124.8 acres of light industrial use, 115.8 acres of light industrial/open space use, and 251.2 acres of heavy industrial/open space use. The open space description is used to designate certain areas which contain habitat values such as wetlands, vernal pools and special status plant species. Therefore, the exact yield of industrial land and location of industrial uses will depend on site-specific biotic surveys. Located in the Village Core, the commercial center is intended to provide retail goods and services, as well as business/professional uses, to meet the frequently recurring needs of the area residents while also providing employment opportunities. Outside the Village Core, the NCSP designates approximately 110 acres of general commercial and light industrial uses along State Route 99.

The zoning and land uses on these lands are not proposed for a change as a result of this specific plan, and much of this land is already developed.

Parks, Open Space, and Public Uses

The NCSP provides for open space and recreational uses comprising roughly one acre for every six acres within the overall Plan area. Between 412 and 580 acres will be devoted to either natural or improved open space, including a neighborhood park, greenbelts, trail corridors, and wetland preserve areas. These areas would be generally available for varying levels of passive to active recreational uses. A 10-acre neighborhood park site is located in the Village Core, directly adjacent to the elementary school site, thus allowing for easy facilitation of joint use. In addition to the Village Core neighborhood park, one 5 acre park is proposed. These will provide areas for organized sports and picnic areas to serve surrounding neighborhoods. A 24.7-acre community park is proposed along Eaton Road south of Sycamore Creek and is intended to serve an area of the region beyond the boundaries of the North Chico Specific Plan.

Non-recreational areas, including the wetland preserve areas and portions of the natural waterway corridors, will be maintained through the CSA for drainage and as multipurpose trails connecting to the Village Core and other facilities of the NCSP area. Open space lands throughout the Plan area are intended to protect natural and cultural resources (wetlands, riparian vegetation, major drainages, and sensitive cultural resources), and to contribute to the rural character of the area.

General Policies

1. Create a functional and attractive residential community, with development of a variety of housing types to accommodate a broad range of household needs complimented by cultural amenities and all public facilities and services necessary to support the population which will result from Plan development.

NORTH CHICO SPECIFIC PLAN

2. Create integrated open space, parks and recreational amenities which will result in improved quality of life for residents of both the Plan area and greater Chico area.
3. Provide commercial goods and services, and employment opportunities which will meet the needs of area residents and reduce the need for daily travel outside the Plan area.
4. Develop a circulation system which provides for the efficient and uncongested movement of vehicles, pedestrians, and bicyclists with minimum intrusion upon the rural and residential character of the area.
5. Increase the mobility of the residents through development of a transportation system which includes automotive and non-vehicular transportation.
6. Develop a community of compatible and harmonious architectural and landscape design.
7. Establish a balanced, pedestrian-oriented Village Core which supports a variety of uses and provide a focal point for the area.
8. Provide public and community services which both adequately serve the community and are cost effective.
9. Preserve the semi-rural lifestyle of the Plan area, while providing for the future housing needs.
10. Protect the long-term operations of the Chico Municipal Airport by providing compatible land uses, adequate setbacks, aviation easements, signing, and other measures.

Agricultural Policies

1. All existing orchards shall be permitted to continue, without interference or interruption by development.

Residential Policies

1. Provide buffers between residential uses and industrial uses and/or arterial thoroughfare streets by using landscaped corridors, berms, greenbelts, and/or setbacks.
2. Preserve and enhance the existing land use patterns and densities north of Mud Creek.
3. Develop a pedestrian-oriented Village Core which provides shopping, social and recreational facilities for the Plan area.
4. Locate multi-family residential uses in and adjacent to the Village Core to maximize the

NORTH CHICO SPECIFIC PLAN

benefits of community amenities and to minimize travel within the development.

5. Provide for a mix of land uses (residential, retail and public) in the Village Core.

Commercial/Industrial Policies

1. Locate and orient commercial buildings to minimize negative impacts on adjoining residences. Outdoor activity areas and noise-generating equipment should be located away from residential areas.
2. Incorporate outdoor public gathering areas, including fountains, arbors, seats, public art, or other elements, into commercial sites to facilitate social interaction by Plan area residents and employees.
3. Identify opportunities to integrate economically beneficial activities such as the designation or improvement of industrial land or the removal of constraints to the development of existing industrially-zoned land adjacent to the Plan area and westerly of the airport.

Parks and Open Space Policies

1. Provide open space, parks and recreational facilities proportionate to the needs of the area and coordinate as appropriate with other Plan elements including wetland and riparian habitat protection, educational facilities, and natural resources.
2. Design parks with recreational amenities and facilities consistent with the needs of nearby residents.
3. Where possible, locate park sites adjacent to school sites, with joint-use agreements with the school district. In such instances, recreation amenities, including play equipment, should be coordinated to minimize duplication.
4. Link parks and nature preserve areas by a pedestrian and bicycle circulation system to the maximum extent possible.

Implementation Measures

Section 7 of this Specific Plan provides development regulations and design guidelines for future development. The design guidelines focus on development within the Village Core and commercial areas. Additionally, Airport Protection Measures are presented, to ensure the long-term compatibility of the Chico Municipal Airport with adjacent development. Development requirements for recreation and open space uses are detailed in the Development Regulations and Design Guidelines, as well as the Parks and Open Space Element.

4. CIRCULATION

Existing Traffic Conditions

The North Chico Specific Plan (NCSP) area currently is served by State Route 99, Eaton Road, Hicks Lane, Garner Lane, Keefer Road, and secondarily, Cohasset Road as shown on Figure 4-1. State Route 99 (SR99), the primary north/south arterial in the area, is a two lane highway adjacent to most of the Plan area, with numerous driveways which serve both residential and commercial uses. Just south of the signalized intersection with Garner Lane, SR99 transitions to a four lane, controlled access freeway.

There are three primary intersections on State Route 99 which serve the Plan area: Keefer Road, Garner Lane, and Eaton Road. The Keefer Road/SR99 intersection has stop signs for traffic entering State Route 99. The Garner Lane/SR99 intersection is fully signalized while the Eaton Road/SR99 intersection is characterized by a full diamond interchange with stop signs at the off ramp/Eaton intersections. The Wilson Landing intersection with SR99 is marked with a stop sign for eastbound traffic turning onto SR99, as is the Esplanade/SR99 intersection.

Access within the Plan area is provided by Garner Lane, Hicks Lane, and Keefer Road. Garner Lane is a north/south two lane roadway with a variable developed width of approximately 24 feet in areas of new development and approximately 18 to 20 feet elsewhere. Existing traffic volumes indicate that Garner Lane serves as a primary access route to and from Chico for existing development in the NCSP area. Hicks Lane is a secondary north/south two lane roadway which also serves as a primary north/south route to and from Chico, with a variable developed width of approximately 16 to 24 feet. Keefer Road is an east/west two-lane roadway with a variable developed width of approximately 20 to 28 feet. Existing traffic volumes indicate moderate use of this route to access SR99. Keefer Road provides the only existing continuous east/west route through the Plan area, including some through traffic from Cohasset Road. The levels of service (LOS) on area roadways and intersections are generally very good (i.e., LOS A) with the notable exception of the left-turn movement from Keefer Road at the Keefer Road/SR99 intersection, which is currently operating at Level D for the AM peak and Level C for the PM peak.

Circulation Concept

The overall circulation concept for the NCSP area is development of a circulation pattern which provides adequate levels of service, a reasonable and convenient access pattern, and recognizes and corrects existing and potential access problems. The circulation system for the Plan area was developed based on results of a traffic analysis prepared for the proposed NCSP land use plan.

The analysis identified projected traffic generation and evaluated road and intersection operation

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under existing and buildout conditions. Technical background data regarding traffic conditions is provided in the *North Chico Specific Plan Draft Environmental Impact Report*. The resulting roadway classifications, standards, and planned improvements identified in the traffic analysis are described below.

The plan provides for the safe and efficient movement of vehicles, bicyclists, pedestrians, and equestrians. A key component of the Plan is integration of land uses to minimize commute traffic and to provide a system of trails to encourage pedestrian and bicycle travel. The close proximity of residential, commercial, industrial, and grade school uses allow for greatly increased pedestrian and bicycle trips in place of automobile trips. The plan emphasizes the separation of automobile traffic from other modes of transportation increasing pedestrian/equestrian/bicycle safety and reducing automobile conflicts and usage.

A variety of street widths and designs are included to accommodate a range of anticipated traffic volumes in a manner compatible with adjacent land use. The Plan and street sections have also been designed to reflect and preserve the rural character of the area. Only in the area immediately adjacent to the Village Core and south of Sycamore Creek are standards proposed that reflect an urban character, and those standards have been designed to produce a village or small town atmosphere. Street sections have been designed to be consistent with underlying Butte County standards and to maintain the existing rural character of the area.

Street System

The NCSP provides for four types of streets and three types of paths and trails:

Streets

- Arterial streets
- Collector streets (rural and urban types of both two and four lanes)
- Local streets (rural and urban)
- Alleys

Paths and Trails

- Paved paths not adjacent to streets
- Paved paths adjacent to streets
- Dirt or gravel trails

Figure 4-1 illustrates the circulation components for the plan area, including generalized roadway alignments and number of lanes. A summary of primary roads and design standards is presented below. Detailed road standards and cross-sections are presented in Section 7 of this Plan.

Arterial Streets

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Arterial streets are at the top of the street hierarchy system. Arterial streets are the widest streets and carry the most traffic. Driveway access to these streets should be limited as well as the number of intersections. When this concept is followed, conflicts in circulation are minimized and the carrying capacity of the street is maximized.

Within the proposed Plan Garner Lane, Keefer Road, and Hicks Lane are proposed to be upgraded to arterial streets. Eaton Road, adjacent to the southerly boundary of the Plan area, is currently planned as a four lane arterial and projected to extend westerly from the present terminus just west of the Esplanade to State Route 32.

The major new circulation feature within the NCSP area will be the new arterial street originating at Hicks Lane in the Village Core and extending westerly over Mud Creek to SR99. A realigned and widened Hicks Lane will serve as the primary entry to the Plan area from the south. The industrial and employment centers of the Plan will be served by a loop road collector extending easterly from Hicks at Caballo and rejoining Hicks in the Village Core. The new circulation network is designed to prevent new, direct access from individual properties and to minimize cross-traffic turning movements.

With a single exception, all arterial streets will operate adequately with two lanes based on the results of the traffic analysis. As the southerly access road to the Plan Area, Hicks Lane between Eaton Road and the Village Core will require 4 lanes. Street sections for this road as well as other arterials, collectors and local streets are described in Chapter 7.

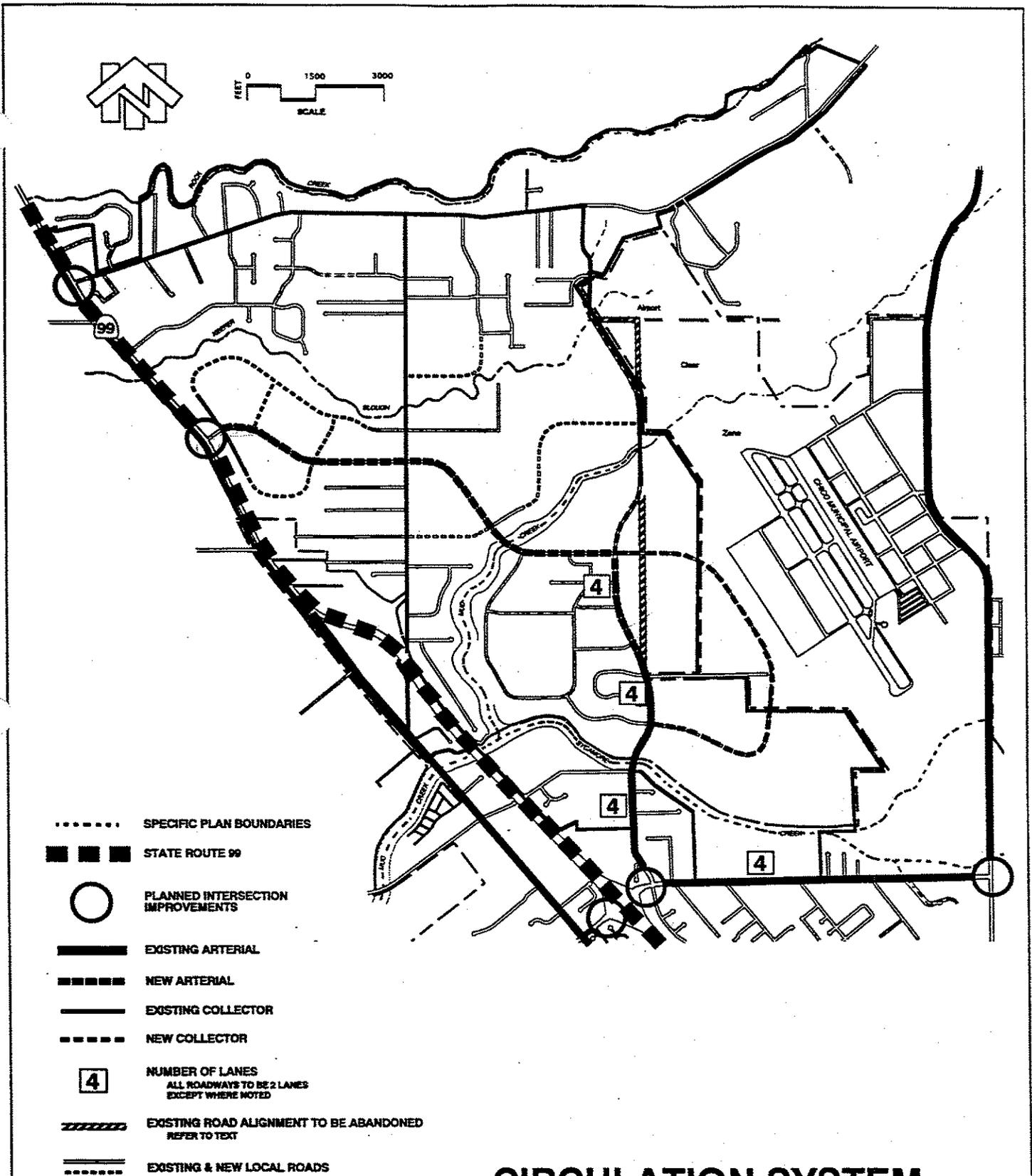
Collector Streets

Collector streets provide access from local streets to arterial streets and also serve as residential, commercial, and industrial streets. Street trees planted in parkways will create shaded streets. Street design will include concrete curbs and gutters with a parkway and sidewalks replicating a small town character. In larger lot areas where 1 acre and greater lots are proposed gravel shoulders and drainage swales may be used in lieu of curb and gutter, except where drainage concerns demand more urban facilities.

There are several collector streets that have somewhat higher traffic counts than local streets but not enough to warrant a full arterial section. Within the Plan area these include the Village Core loop road and other connecting streets to Hicks Lane, the new connecting road serving the industrial areas, and Sycamore Lane.

Local Streets

Local streets provide the access to the various areas of the Plan. In the residential area they provide access to homes; in the commercial and industrial area to shopping and jobs. These streets are generally characterized by numerous driveways in residential areas and project entrance driveways in commercial and industrial areas. Local streets are designed to connect to the Collector Streets eventually leading to the Arterial Street system.



CIRCULATION SYSTEM

Figure 4-1

North Chico Specific Plan

NORTH CHICO SPECIFIC PLAN

Local residential streets provide access to homes. In the Village Core, local streets will have the same character as urban collector streets, with sidewalks and curbside landscaped parkways creating a traditional tree-lined street replicating the small town character.

Local streets within commercial areas will be designed to not only provide access through the commercial area, but also to provide direct access to stores. Diagonal parking designed in conjunction with the local street will be provided on both sides of the street with larger sidewalks and street tree planters.

Local streets in the industrial areas are designed to provide for large truck turning movements into driveways and will provide areas for parallel on-street parking. Required landscaped parkways, sidewalks, curbs and gutters will continue the overall NCSP street design theme.

In the remainder of the Plan area, local rural streets may be built as narrow as 24 feet (paved width) to maintain the rural character of the area.

Special Streets

Alleys may be utilized in the Village Core where curved roads make pulling in and out of driveways unsafe and where driveways and garages taking access from the front would dominate and overpower the street. Alleys would be used for access to parking in the rear and allow for a much more attractive street. Alleys will have a 24 foot paved section with 2-foot planting strips bordering each side. They will be designed with a standard street crown section and not permit a center drainage area.

The Plan provides for numerous cul-de-sac streets in both rural and urban residential areas. All cul-de-sacs will have to meet Butte County standards in order to accommodate emergency vehicles, trash truck and the like.

Planned Road and Traffic Improvements

In order to provide an adequate circulation system through the Plan area, a number of road and intersection improvements will be necessary, as summarized on Table 4-1. As indicated above, a new arterial link will be constructed between Eaton Road and Highway 99.

Eaton Road, a major arterial bordering the Plan area on the South, will require widening to four lanes, with widening of the SR99 overpass and modifications of the interchange at this location. Both this widening to four lanes and interchange modifications are planned improvements of the City of Chico and County of Butte and are to be funded through the Chico Urban Area Street Facility Fees.

As development occurs, existing roadways will be widened and improved to meet road standards outlined in Section 7. Construction of widened and new bridges over Sycamore and Mud Creeks will be required as part of construction of the arterial link between Eaton Road and Highway 99.

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Existing intersections will require some modifications, especially along Eaton Road where signals will be required at the on/off ramps of State Route 99, the New Arterial road, and Cohasset Road. Due to the proximity of Hicks Lane to the Eaton/SR99 interchange, the Plan requires traffic control on Hicks Lane to restrict turning movements to right-turns in and out, only. Internal road traffic controls include multiway stop at the intersections of realigned Hicks Road/New Arterial and Garner Lane/New Arterial. Stop sign control will be provided on the westbound approach of the New Arterial at its intersection with SR99.

Trail System

A system of interconnected trails, paths, and sidewalks is incorporated into the Plan to facilitate travel within and beyond the Plan area by pedestrians, bicyclists, and equestrians. The routes are intended to not only provide an alternative to automobile travel but also to foster health and social interaction among residents.

Class I bike paths and trails are planned to accommodate a mix of travel and recreational modes. Class I paths occur adjacent to major roadways and through greenbelts. Recreational trails are located primarily along major drainages. Figure 4-2 illustrates locations of both Class I paths and trails in the Plan area. Class I paths along roadways occur along segments of the new arterial, on Garner Lane, and on segments of key collector streets.

Additionally, a Class I path through the Village Core greenbelt and along the old Hicks Lane right-of-way provides primary access through the Plan area and points south. A number of paths branch off of this path to provide for direct bike and pedestrian circulation away from and generally uninterrupted by auto traffic. The paths would be a minimum of 8 feet wide and would meet CalTrans standards for Class I paths.

It should be noted that the *Chico Urban Area Bicycle Plan* identifies bicycle trails in the NCSP area. The *Bicycle Plan* was prepared by the Community Transit Advisory committee, consisting of representatives from both the City of Chico and Butte County. Within the NCSP area Class I separated bike paths are proposed along Sycamore Creek, along portions of Mud Creek, and east of the NCSP area in the vicinity of the Airport. The proposed Specific Plan is consistent with, and implements, the Class I paths proposed in the Chico Bicycle Plan.

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**TABLE 4-1
SUMMARY OF NORTH CHICO SPECIFIC PLAN TRAFFIC IMPROVEMENTS**

Location	Condition		
	Existing + Project	Cumulative (2007)	Cumulative + Project
SR99/Keefer	<input type="checkbox"/> Add right turn lane on NB SR99 <input type="checkbox"/> Add left turn lane on WB Keefer	<input type="checkbox"/> Same	<input type="checkbox"/> Same
Keefer/Garner	<input type="checkbox"/> None	<input type="checkbox"/> None	<input type="checkbox"/> None
Keefer/Hicks	<input type="checkbox"/> None	<input type="checkbox"/> None	<input type="checkbox"/> None
SR99/Garner	<input type="checkbox"/> None	<input type="checkbox"/> None	<input type="checkbox"/> None
Esplanade/Garner	<input type="checkbox"/> None	<input type="checkbox"/> None	<input type="checkbox"/> None
Eaton/SR99 (SB ramps)	<input type="checkbox"/> Install signal <input type="checkbox"/> Add EB thru lane <input type="checkbox"/> Add WB left turn lane	<input type="checkbox"/> Install signal <input type="checkbox"/> Add WB left turn lane	<input type="checkbox"/> Install signal <input type="checkbox"/> Add EB thru lane <input type="checkbox"/> Add WB left turn lane
Eaton/SR99 (NB ramps -Hicks)	<input type="checkbox"/> Install signal <input type="checkbox"/> Restrict Hicks to right turn in and out only <input type="checkbox"/> Add second right turn lane on SB Hicks <input type="checkbox"/> Add EB and WB thru lane on Eaton <input type="checkbox"/> Add WB right turn lane on Eaton <input type="checkbox"/> Add EB left turn lane on Eaton	<input type="checkbox"/> Install signal <input type="checkbox"/> Add WB right turn lane <input type="checkbox"/> Add EB left turn lane	<input type="checkbox"/> Same as Existing + Project Condition
Eaton/Cohasset	<input type="checkbox"/> Install traffic signal <input type="checkbox"/> Install left turn lane, thru lane and thru-right lane on all approaches	<input type="checkbox"/> Same	<input type="checkbox"/> Install traffic signal <input type="checkbox"/> Install left turn lane, thru lane and thru-right lane on WB approach and SB approach <input type="checkbox"/> Install left turn lane, two thru lanes and right turn lane on EB approach <input type="checkbox"/> Install two left turn lanes, one thru lane and a thru-right lane on NB approach
New Hicks/Eaton/ Intersection	<input type="checkbox"/> Install traffic signal <input type="checkbox"/> Install two left turn lanes, one thru and one right turn lane on SB approach <input type="checkbox"/> Install one left and one thru-right turn lane on NB approach <input type="checkbox"/> Install one left, thru and right turn lanes on WB approach <input type="checkbox"/> Add two left and one thru-right turn lane on EB approach	<input type="checkbox"/> None	<input type="checkbox"/> Same as Existing + Project
Eaton	<input type="checkbox"/> Widen to four lanes - new road to Esplanade	<input type="checkbox"/> Widen SR99 overcrossing to three lanes	<input type="checkbox"/> Widen to four lanes from Cohasset to Esplanade
New Road (Eaton to Hicks)	<input type="checkbox"/> Widen to four lanes	<input type="checkbox"/> None	<input type="checkbox"/> Widen to four lanes

Trail corridors are proposed along existing creeks and drainages, and proposed drainage areas. These corridors, in conjunction with Class I paths, provide an interconnecting system of pedestrian, bicycle, and equestrian travel. Trails are intended to link lower density residential areas with the Village Core and industrial areas, and serve as a place to walk, jog, ride bikes, or ride horses. These trails will be 8- to 14-foot wide. Section 7, Development Regulations, should be referred to for additional path and trail information relating to design, landscaping, lighting, etc.

Public Transit/Transportation System Management

4-6 Circulation Element

NORTH CHICO SPECIFIC PLAN

The use of various modes of public transit, including buses and carpooling, will lower automobile trips within the Plan area.

Circulation Goals and Policies

1. Develop a circulation system which provides for the efficient and uncongested movement of vehicles, pedestrians, and bicyclists with minimum intrusion upon the rural and residential character of the area. Traffic on all streets within the Plan area should not exceed a Level of Service (LOS) "C". In no case shall the LOS exceed "D".
2. Increase mobility of residents through development of an adequate, balanced transportation system that includes automotive and non-automotive transportation uses

Circulation Policies

1. Limit access from residences and businesses along major arterials to a general spacing of 1 access point for each 300 feet. Utilize reciprocal access agreements, shared driveways, easements and other methods. In no case is it the intent of this policy to deny vehicular access to any legal lot.
2. Design local streets within residential neighborhoods to discourage automobile and truck through-traffic by use of cul-de-sac streets or other design techniques.
3. In order to encourage use of the proposed paths/trail system, design street patterns which utilize single loaded streets and other street and lot design techniques along proposed paths/trails.
4. Where cul-de-sac streets are proposed, utilize designs which will provide pedestrian/bicycle (and equestrian where appropriate) access. This access should lead to or connect to the overall planned path/trail system. Key lots (lots located at the end of cul-de-sac streets) should be minimized.
5. In order to encourage public transit usage, the arterial street design shall provide for bus turnouts and for the location of bus shelters.
6. All path and trails designated within properties which are proposed for subdivision shall be required to be designed and improved by the developer of the subdivision in accordance with the requirements of this Plan and good planning and engineering practice. Maintenance of these areas will be the responsibility of CSA 87.
7. Encourage non-vehicular access through the Plan area and to the commercial and industrial areas by requiring the construction of sidewalks, pedestrian/bicycle paths and

NORTH CHICO SPECIFIC PLAN

bicycle parking facilities.

8. Enhanced street entries shall be required at the following location:
 - a. The new arterial/Highway 99.
 - b. The north and south entry to the industrial area.
 - c. The new Hicks Road/Eaton Road.
 - d. The new arterial near Mud Creek.
 - e. The new arterial near Sycamore Creek.

The entry shall consist of landscaping and hardscape. Rock or block walls shall be installed behind the sidewalk. The name of each land use area shall be designed into the entries such as, "Industrial Park," and the like. Shrubbery and at least four specimen size trees shall be used to further enhance the entry. Each entry area shall also have a raised landscaped median with a left turn pocket. The design of the street entries shall be approved by the Directors of Development Services and Public Works.

9. A detailed alignment study for Old Hicks Road/New Hicks Road and Eaton Road shall be prepared by or under the direction of Butte County. The study shall indicate the precise engineering of the (right-of-way) alignment, the removal or relocation of structures, the installation of curb, gutter, sidewalk, and other infrastructure necessary to accomplish the realignment.
10. Noise attenuation along existing and proposed arterials shall be required to protect residential development proposed to be located adjacent to the proposed arterials. Noise attenuation measures shall be required to reduce interior noise levels to 45 dB for proposed residential development adjacent to existing or planned arterials and/or when adjacent to Highway 99.
11. No new street alignments shall exclude safe and convenient access to properties. County staff shall pursue negotiations with CalTrans for appropriate traffic control, including, but not limited to, traffic signals and street alignments along Highway 99, primarily at the intersections of Keefer Road and the new arterial.

5. PARKS, OPEN SPACE, AND RESOURCE MANAGEMENT

Existing Park and Recreation Facilities

Park and recreation services and facilities in the Chico Urban Area are provided by the Chico Area Recreation and Park District (CARD) and the City of Chico Parks Department. There are currently no park or recreational facilities in the North Chico Specific Plan (NCSP) area. The closest recreational facility to the area is the Pleasant Valley Recreation Center located on North Avenue approximately 2 miles from the southernmost end of the Specific Plan area.

It is the intent of this Plan to have the park and open space facilities managed by CARD, funded through CSA 87 or other funding mechanisms.

Park and Open Space Concept

The North Chico Specific Plan provides for an appropriate variety of recreational and open space uses within the Plan area. In total, approximately 40 acres of the Plan area will be devoted to park land and between 412 and 580 acres of land will be devoted to open space.

There is one type of park proposed in the Plan area, i.e. neighborhood park. It is the intent that two neighborhood parks will serve the needs of the existing and future residents of the area. All of the parks are proposed to be located adjacent to the open space/trail corridors in order to facilitate easy access and usage.

There are four basic functions of open space including parks, path and trail corridors, natural open space areas for the protection of wetlands, and drainage corridors. These areas will be available for varying levels of passive to active recreational uses. Non-recreational areas, including wetland preserve areas and portions of the drainage corridors, will be managed for resource values such as riparian habitat, seasonal wetlands and vernal pools. Collectively, open space lands throughout the Plan area are intended to protect natural and cultural resources (wetlands, riparian vegetation, major drainages, and sensitive cultural resources), and to maintain the rural character of the area.

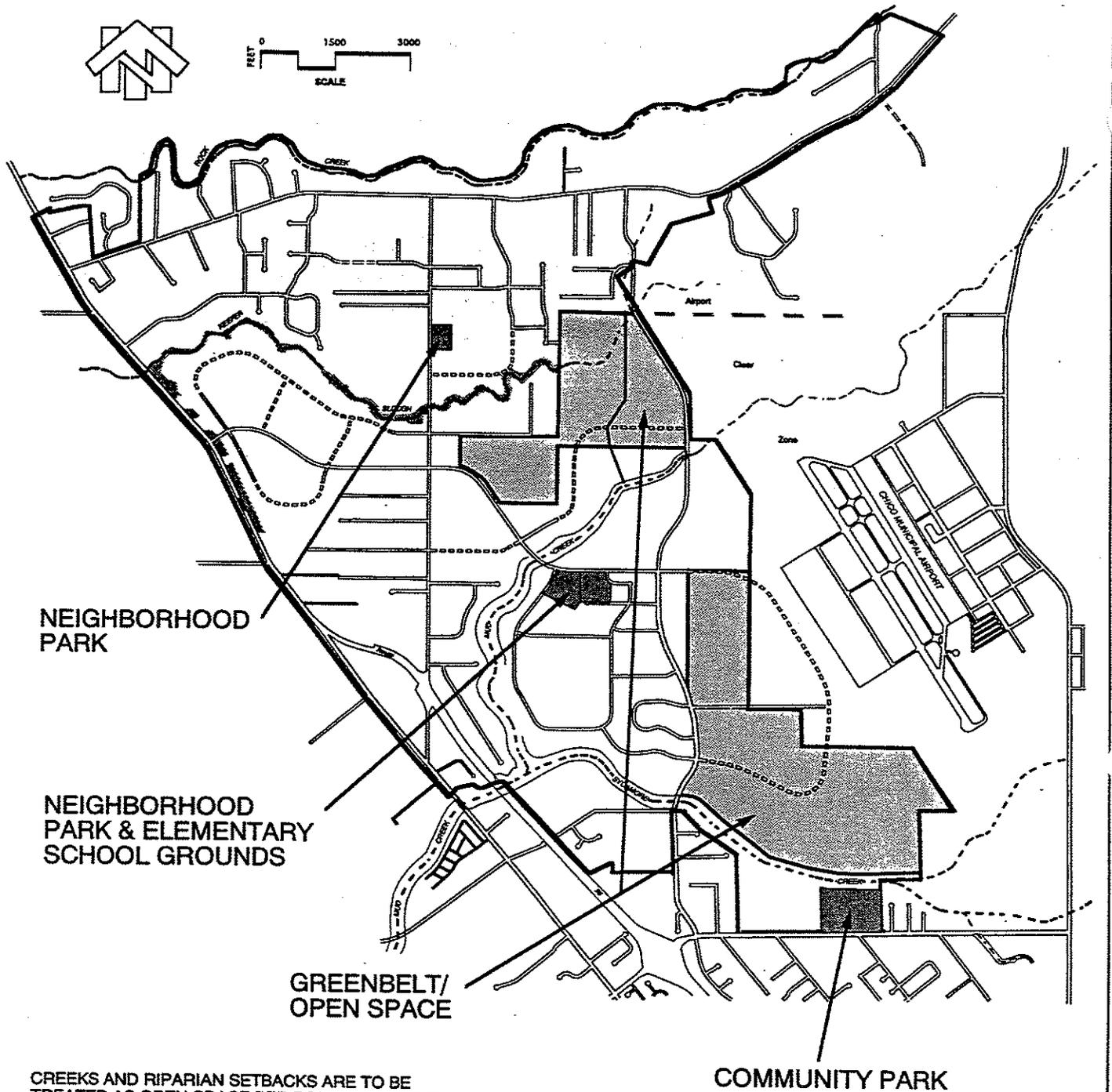
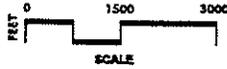
Plan area open space and park areas are illustrated in Figure 5-1. Table 5-1 summarizes the planned open space uses by acreage and anticipated uses. This element of the NCSP details the proposed open space uses identified above. Additionally, Section 7, Development Standards and Design Guidelines, further defines the nature and use of open space areas.

Parks

NORTH CHICO SPECIFIC PLAN

Neighborhood Park Site: The proposed 14.8-acre neighborhood park site will be located in the Village Core, adjacent to the elementary school site and adjacent or near the neighborhood shopping district.

The park will provide primarily active recreational uses with some passive use areas. Based on the 1988 *Park and Recreation Plan* for CARD, facilities that could be provided in a neighborhood park include a playground, picnic facilities and shelter, trails, open space and nature areas, restroom, tennis courts, a basketball court and a multi-use open field.



CREEKS AND RIPARIAN SETBACKS ARE TO BE TREATED AS OPEN SPACE PER POLICIES IN TEXT.

SEE NOTE ON FIGURE 3-1 REGARDING SPECIFIC BOUNDARIES OF "GREENBELT / OPEN SPACE".

PARKS & OPEN SPACE

Figure 5-1

North Chico Specific Plan

NORTH CHICO SPECIFIC PLAN

**TABLE 5-1
SUMMARY OF OPEN SPACE USES**

Open Space Category	Allowable Uses/Facilities	Acres (Approx.)
Community Park	<u>Active and passive recreational opportunities.</u> Facility types include: play fields; tennis courts; basketball court; picnic areas; children's play area; restrooms; and similar facilities as determined by the Chico Area Recreation and Park District.	24.7
Neighborhood Park #1	<u>Active and passive recreational opportunities.</u> Facility types include: play fields; tennis courts; basketball court; picnic areas; children's play area; restrooms; and indoor recreational facility (facilities to be coordinated with adjacent elementary school).	10.0
Neighborhood Park #2	<u>Active and passive recreational opportunities.</u> Facility types include: play fields; tennis courts; basketball court; picnic areas; children's play area.	5.0
Path and Trail Corridors	<u>Passive and active recreational opportunities and non-automobile circulation.</u> Facilities include: trail system connecting Plan area neighborhoods to Village Core; and open drainage channels.	204.1
Natural Resource Areas	<u>Natural resource protection and passive recreational opportunities.</u> No facilities recommended. Acreage will vary based on site-specific biotic surveys. (Estimated acreage range is 1/3 to 2/3 of land use designations M-1/OS, M-2/OS and SR-1/OS.)	169-336
Total Acreage		412-580

Note: All open spaces will function in several categories. Acreages are listed for the primary use only.

The proposed neighborhood park area provides major open space large enough for a full-sized soccer field and softball field.

Additionally, tennis courts, basketball facilities, picnic area and shelter, children's play area, indoor recreational facility (which will be sized to allow for a community meeting room), restrooms, and parking could be accommodated on-site.

The village core park facilities will be coordinated with recreational facilities usually provided within an elementary school setting. The layout and design of the park and park facilities will allow for its use by the general public when school is in session.

The elementary school and neighborhood park will be easily accessible by automobiles, pedestrians, bicyclists, and equestrians. As indicated in Section 4, Circulation Element, new roads through the Plan area will provide access routes into the school/park site via trails, bike paths, bike routes, and sidewalks. School children and other local residents from surrounding neighborhoods will be able to access the school/park site, as well as adjacent village shopping area, on foot, bicycle, or horse along an interconnecting system of paths and trails (see Figure 4-2). The NCSP also allows for a stable site north of Mud Creek, near the new bridge crossing. The stable would help to encourage the continued use of horses as a means of

transportation throughout the Plan area.

An additional five acre park is planned along Garner Avenue near the planned fire station. The intent is to utilize this park site for active recreation. It should include facilities for soccer, little league and other team sports.

Path and Trail Corridors: As described in Section 4, Circulation Element, the NCSP provides for an interconnecting system of paths and trails (see Figure 4-2). This system not only provides for and encourages alternative transportation modes, but accentuates recreational opportunities in the Plan area and beyond. Trails and paths will connect area residents with Plan area parks and ultimately to recreational/park facilities outside the Plan area. Proposed trail and path corridors also provide areas for drainage and occasional stormwater detention areas (see Section 6, Drainage).

The NCSP provides for path/trail corridors along Mud and Sycamore Creeks, consistent with recommendations contained in the *CARD Park and Recreation Plan* (September 1988). The CARD Plan identifies portions of these creeks as future lineal parks that would provide for an interconnecting trail system around the greater Chico area. The *Park and Recreation Plan* stipulates that lineal parks are generally open space areas and/or trail systems that follow a stream corridor, ravine or other elongated feature. This type of park often contains pedestrian and/or bicycle trails.

The NCSP provides for a pathway corridor and Class I path along that portion of Sycamore Creek that crosses the Plan area.

Along Mud Creek and through the Village Core greenbelt, trail and path corridors will provide connections from the proposed neighborhood park centrally located within the Plan area, to the Chico Loop which will ultimately surround the 23-mile Chico Urban Area.

Section 7, Development Standards and Design Guidelines, provides a description and illustrative cross-sections of path and trail corridors.

Community Park Site: There is a proposed 24.7-acre community park site located in the southeastern corner of the Plan Area. Though not originally programmed for the Specific Plan, the site proved ideal for the long-term planning objectives of the Chico Area Recreation and Parks District (CARD). As such, the site will serve the broader Chico community generally north of Lindo Channel. According to the 1994 Chico General Plan, *community parks* serve all ages and may include facilities for low-intensity/passive recreation use, lighted fields, courts, swimming pools, and areas and buildings for community festivals and civic events, as well as for organized sports and athletic competitions. Generally, restrooms and some off-street parking are provided; community parks are usually 20 acres or larger.

Open Space For Natural Resource Management

NORTH CHICO SPECIFIC PLAN

Up to 500 acres of open space areas are designated throughout the Plan area to protect natural and cultural resources. Such resources include: wetlands, major drainage ways, riparian vegetation, special status species habitat, and sensitive archaeological resources. These areas are generally shown on the Land Use Map, Figure 3-1, and on the Parks and Open Space Map, Figure 5-1. It should be noted that these are general locations and will need to be precisely located and set aside in conjunction with proposed development.

Wetlands

Several types of wetlands currently exist in the Plan area including, but not limited to perennial/seasonal streams, vernal pools, ephemeral drainages, and seasonal ponds. Development of any area which directly or indirectly impacts these or other identified wetlands may be subject to a review permitting procedure under Section 404 of the Clean Water Act administered by the U.S. Army Corps of Engineers (COE) and other federal and state agencies.

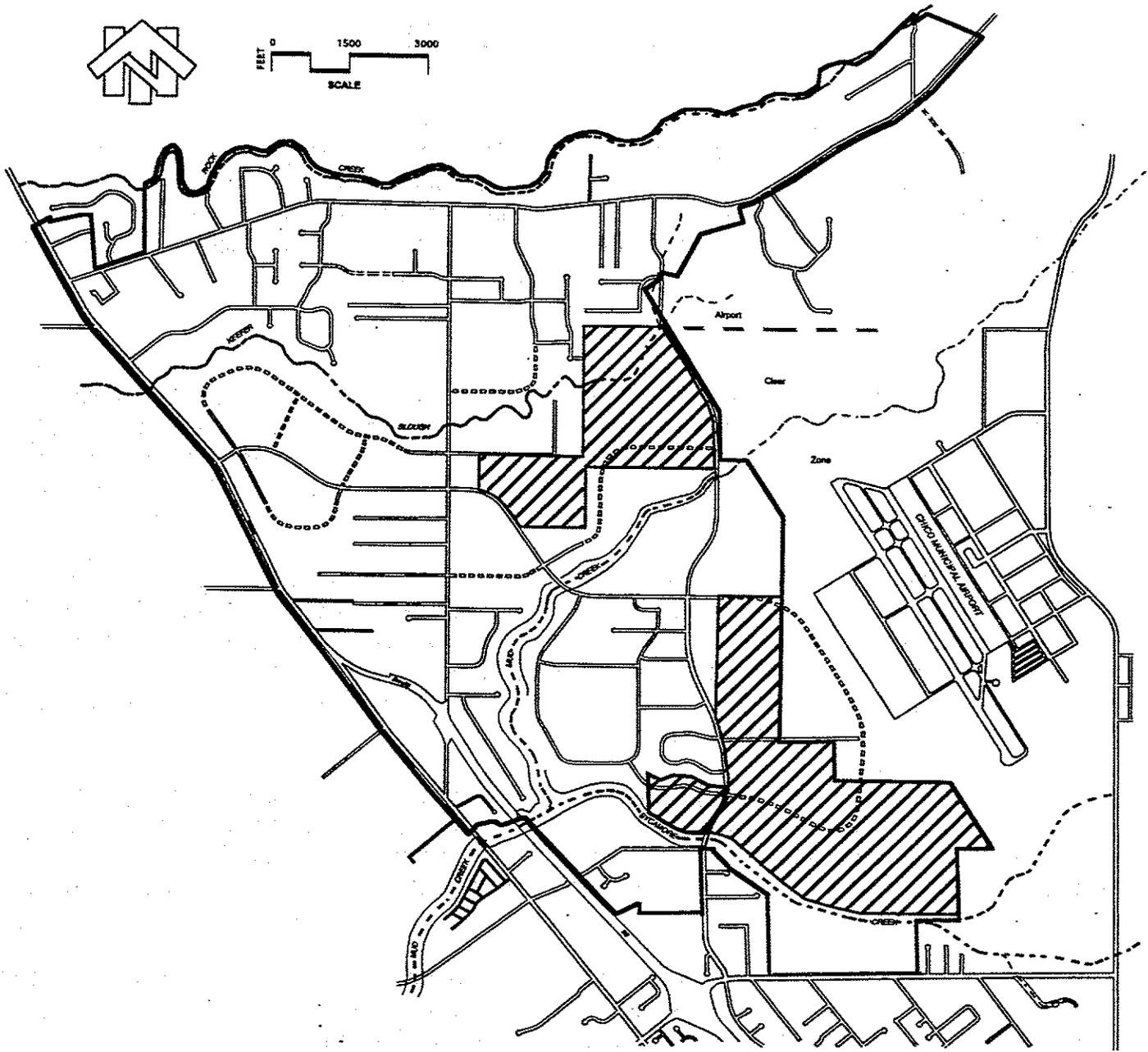
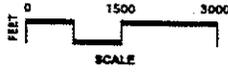
This Plan recommends avoidance of the wetlands as the primary means of mitigation. It should be noted that COE also recommends avoidance as part of the 404 permit process. The NCSP provides where Butte County and COE determines that avoidance is not possible, the developer of the property shall be required to prepare a mitigation plan. The plan shall be based upon "no net loss" to wetland habitat values or acreage, and no adverse impacts upon any state or federally listed sensitive plant or animal species.

Approximately 57 acres of potential jurisdictional wetlands were preliminarily identified through field survey work. Formal wetlands delineation studies were not conducted and must be prepared prior to development of properties where wetlands have been identified or are likely to be found. Figure 5-2 generally indicates those areas where wetland habitat is known to exist and jurisdictional wetlands may occur.

Approximately 49 acres of wetlands are expected to be maintained by designation of three wetland preserve areas, designated as open space areas in the NCSP (see Figure 5-2). The NCSP provides for in-kind compensation for approximately 15 acres of wetlands that could be filled or otherwise impacted as a result of proposed NCSP development. The use of the abandoned gravel pit as a groundwater recharge and detention basin would require a 404 permit.

Other Resources

Major drainage ways within the Plan area (Rock Creek, Keefer Slough, Mud Creek, and Sycamore Creek) will remain primarily unaltered as the drainage ways are currently managed for flood control and are mostly devoid of vegetation (with the exception of the westernmost portion of Keefer Slough). These drainage ways constitute an element of natural open space in the Plan area which, in conjunction with setbacks along specific reaches, will provide for protection of riparian vegetation and habitat, special status species, and sensitive archaeological resources in the Plan area.



AREAS IDENTIFIED AS CONTAINING POTENTIAL WETLAND HABITAT,
AND REQUIRING ADDITIONAL MEASURES INCLUDING COMPLIANCE
WITH THE 404 PERMIT PROCESS.

POTENTIAL WETLAND HABITAT

Figure 5-2

North Chico Specific Plan

NORTH CHICO SPECIFIC PLAN

Parks and Open Space Goals and Policies

1. Create integrated open space, parks and recreational amenities which will result in improved quality of life for residents of both the plan area and greater Chico area.

Parks and Open Space Policies

1. Provide a network of linear open space along identified drainage and creek areas and connecting paths/trails to the Village Core and industrial area.
2. Provide parks and recreational facilities located in order to adequately serve the existing and future residents of the Plan area. Incorporate, as appropriate, wetland and riparian habitat protection, and nature interpretive facilities.
3. Provide a process where the Plan area residents and owners can participate in the design of the parks and the recreational facilities.
4. Locate the neighborhood park sites adjacent to the elementary school sites and encourage CARD and the Chico Unified School District to enter into a joint-use agreement.
5. Require that all park and open space areas maintenance are funded through the amendment of the existing CSA or other method acceptable to LAFCo, Butte County, and the responsible maintenance group or agency.

Environmental Protection Policies

1. Minimize impacts to archaeological, biological, riparian and other natural resources through avoidance.
2. Utilize avoidance as the main form of mitigation of impacts on identified wetlands. Where avoidance is not possible due to the need to provide critical feature of the NCSP, a no net loss concept will be used.

This may require the acquisition of unencumbered wetlands, payment into a wetlands mitigation fund, or other appropriate mitigation in effect at the time of development.

3. If found to be compatible with the protection of wetland resource areas, reserve locations in major wetland preservation areas for possible nature interpretation areas, such as viewing or lookout areas and interpretive walking trails.

Drainage Policies

1. Utilize, to the maximum extent possible, natural or natural-appearing streams or drainage courses for storm drainage.

NORTH CHICO SPECIFIC PLAN

2. Design stormwater facilities, including detention basins, to ensure public safety, to be visually unobtrusive, and to provide and, where feasible, recreational uses.
3. Development within designated floodplains shall conform to County and FEMA regulations. Development shall not be permitted in the floodway. Limited recreational activities, which are not in conflict with flood control, the 100 year floodplain, or habitat uses may be permitted.
4. Utilize pollution control traps or sediment basins, designed pursuant to County standards and/or direction, for storm drainage systems. Minimize releases of concentrated runoff into wetland preserve areas.
5. All drainage areas shall be maintained through amendment to the CSA, or other means acceptable to LAFCo, Butte County, and the maintaining group or agency.

Implementation

Section 7, Development Standards and Design Guidelines, provides specific measures to guide development which, when implemented, will satisfy the "no net loss" of wetlands concept. In general, the standards provide for avoidance, minimization of impact, compensation for unavoidable fills, and protection of existing and created wetlands in the Plan area. Requirements and procedures for pursuing 404 permitting are also stipulated.

Section 7 also provides standards for the protection of other resources, such as riparian vegetation, special status species habitat, sensitive archaeological resources, and major drainage ways. Standards include protection through avoidance of resources and setbacks from resources.

6. PUBLIC FACILITIES AND SERVICES ELEMENT

Introduction

Public facilities and services are one of the more important components of the Plan. This element covers water services, wastewater disposal and the alternatives proposed, stormwater management, schools, fire protection, solid waste, and utilities.

Water Service

Existing Conditions: The North Chico Specific Plan (NCSP) area currently is not served by a domestic water company or public agency. Individual residences and businesses in the area rely on private wells for their domestic water needs.

Water service for the greater Chico area is provided by the California Water Service Company. California Water currently serves the airport immediately east of the NCSP area and developed residential areas south of Eaton Road.

The Company relies on groundwater for its supply and operates 62 deep wells and 5 storage tanks in the Chico water service area. The wells typically can supply approximately 1,000 gallons per minute (gpm). Historical water demands have required 1 well for every 500 residential units or a peak hour demand average of 2 gpm per residential unit. Water is distributed through a 6-inch and 8-inch diameter pipe grid system, with a few 12-inch diameter pipes to serve commercial and multi-family developments. The existing system can supply about 89 million gallons per day (mgd); existing peak demand has reached 40 mgd. The average demand on the water distribution system is 19 mgd.

Proposed Facilities: Domestic water service, for a portion of the Plan area, is proposed to be provided by California Water Service Company. The Company will require expansion of its service area to include the Village Core and other sewered portions of the NCSP area. The maximum day water demand for this proposed service area is 4,364 gpm, as shown on Table 6-1, with a peak hour demand of 6,546 gpm, based on a peaking factor of 1.5. Average day demand is approximately half of maximum day demand or 2,182 gpm.

Water supply for the Plan area will be met entirely from deep aquifer wells. A minimum of 7 wells capable of producing 1,000 gpm will be needed to meet the estimated peak demand of 7,050 gpm. The service area will be served by a well grid network. A well grid network does not require trunk transmission mains to deliver water from one area to another. Wells are located in close proximity to the demand source. Therefore, peak hour demand will be met completely from wells. The proposed distribution facilities will be sized to meet maximum day demand.

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**TABLE 6-1
MAXIMUM DAY WATER DEMAND**

Land Use	Acreage/Units (Village Core/Sewered)	Max. Day Demand Gallons Per Minute	Total Maximum Day Demand Gallons Per Minute
R-1: Single Family Residential	912 units	1.7	1,550
R-2: SF/MF Residential	829 units	1.4	1,161
R-3: Multi-Family Residential	308 units	1.0	308
Commercial/Office	50.6 ac	3.1	157
Heavy Industrial	125.6 ac ⁽¹⁾	3.1	389
Light Industrial	182.7 ac ⁽²⁾	3.1	566
Parks	39.7 ac	3.7	147
School	8.5 ac	3.1	26
Open Space (to be irrigated)	100 ac	0.6	60
Total			4,364

(1) Estimated net 50% of M-2/OS acreage after natural resource area set aside.

(2) Estimated net 50% of M-1/OS acreage after natural resource area set aside; 100% of M-1 acreage.

Wastewater Disposal

Existing Conditions: The NCSP area is not currently served by wastewater collection and treatment facilities; residences in the area are served by septic systems.

The City of Chico operates the only sewer collection system currently in the area. The wastewater which is collected by the City's system is treated at the Water Pollution Control Plant west of the urban area on River Road, a regional facility that can serve the entire urban area. The plant has recently been expanded to a capacity of 6 mgd and is designed for continued modular expansion to an ultimate capacity of 30 mgd.

The City of Chico Sanitary Sewer Master Plan (December 1985) identifies a major new 33-inch gravity trunkline, approximately 7 miles long, extending from Eaton Road south of the Plan area to the regional treatment plant.

Elevated levels of nitrate in groundwater has become a significant problem in portions of the urban area immediately south of the Specific Plan Area.

Septic tank leachate has been identified by the State Water Resources Control Board, as the major source of nitrates in the shallow and intermediate aquifers, with agricultural activities and urban runoff as the second and third highest contributors, respectively.

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As a result of this contamination, the State Water Resources Control Board has adopted a prohibition order which prohibits the discharge of waste from individual disposal systems installed after July 1, 1990 and the discharge from all existing systems after July 1, 1995. Individual septic systems with less than one unit per acre (334 g/ac/day) are exempt from the prohibition. Currently, only the industrial and adjacent residential area of the NCSP fall within the authority of the prohibition order.

Proposed Facilities: The NCSP proposes sanitary sewer service only in the area southeast of Mud Creek. According to the Butte County Department of Environmental Health, the area north and west of Mud Creek has potential for the use of septic tanks. The proposed land use for the area, one-acre lots or larger, meets the County required minimum lot size for septic systems.

Wastewater Generation Rates

Design flows are based on the City of Chico Sanitary Sewer Master Plan wastewater generation rates and are shown in Table 6-2. The average dry weather flow for the sewered portion of the Plan area is 1.05 mgd. The estimated peak wet weather flow is 2.48 mgd.

TABLE 6-2 WASTEWATER GENERATION					
Land Use	Rate			Specific Plan Buildout	
	Units Per Acre*	People Per Unit	Average Dry Weather Flow-Gallon Per Acre Per Day **	Total Acres	Total Average Dry Weather Flow
R-1	4	3.6	1,150	233.9	269,000
R-2	7	2.3	1,300	118.4	153,900
R-3	18	2.0	2,900	17.1	49,600
Commercial	—	—	1,500	24.6	36,900
Industrial	—	—	1,500	308.3 ^m	462,500
Office	—	—	1,500	26.0	39,000
School	—	—	1,500	8.5	12,800
TOTAL					1,023,700

* Unit flows are for net areas.
 ** Assumes 80 gpcd.
 Peaking Factor = $1.97/Q^{0.19} = 1.9$
 Peak Wet Weather Flow = $(PF \times Q) + (Acreage \times 650) = (1.9 \times 1,052,250) + (740 \times 650) = 2.48 \text{ mgd}$
 SOURCE: City of Chico Improvement Standards
 (1) Estimated net 50% of M-2/OS and M-1/OS acreage plus 100% of M-1 acreage.

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On-Site Facilities

The trunk sewer collection facilities needed to serve the Plan area southeast of Mud Creek include: 3,500 lineal feet (lf) of 10-inch pipe, 4,000 lf of 12-inch pipe, 3,500 lf of 15-inch pipe, and 4,500 lf of 18-inch pipe. The entire area can be sewered by a relatively shallow gravity system. The facilities are shown in Figure 6-1. Cumulative flows are shown in Table 6-3.

It is the policy of the City of Chico to require annexation to the City of all properties connecting to the sewer collection system. Therefore, rather than automatically assign the area to be sewered to be annexed to the City of Chico, an alternative sewer system has been proposed with which annexation would not be an issue.

**TABLE 6-3
PEAK WASTEWATER FLOWS**

Node	ADWF Gal Per Day	PF	G/I Gal Per Day	PWWF Gal Per Day	Recommended Pipe Size
(Main Reach) G	217,455	2.5	102,895	646,533	12-inch
F	229,390	2.5	109,655	683,130	12-inch
E	505,528	2.1	242,158	1,303,767	15-inch
D	575,391	2.1	281,373	1,489,694	15-inch
C	624,841	2.1	309,323	1,621,489	18-inch
B	806,240	2.0	361,583	1,812,565	18-inch
A					
(East Reach) H	140,100	3.0	60,710	481,010	10-inch
I	254,860	2.6	102,560	765,196	
West Reach	71,900	3.0	23,985	190,035	10-inch
Total Flow	1,052,250	1.9	488,128	2,487,403	

It should be noted that if any annexations to the City of Chico occur, it must be done in a manner to continue the financing/funding program for the NCSP unchanged.

Alternative 2 considers the development of a new package treatment plant to service the sewered portion of the Plan area. This alternative may also be available due to studies now underway with regard to groundwater nitrate and potential sewerage of portions of the Chico Urban Area.

NORTH CHICO SPECIFIC PLAN

Alternative 1: Extension of City of Chico Sewer Services

Alternative 1 is based on the adopted City of Chico Sanitary Sewer Master Plan.

Collection: The Sanitary Sewer Master Plan proposes a gravity sewer outfall from Eaton Road, south of the Plan area, to the Water Pollution Control Plant on River Road. The outfall is approximately seven and one half miles long. The design of this outfall pipeline includes capacity for a portion of the proposed sewered area of the North Chico Specific Plan. The City's master plan peak wet weather flow (PWWF) capacity for this line is 3.5 mgd. The projected additional flows for the Plan area is 1.5 mgd, totaling 5.0 mgd. The total NCSP flow is 2.5 mgd. This new total flow would require the construction of an outfall line approximately one pipe diameter larger than identified in the current Master Plan. Based upon percentage of flow contributed, the NCSP area's share of the outfall line cost would be 50 percent. Cost estimates for installation of the trunk line are \$7,702,000 with the NCSP share estimated at \$3,851,000 (see detailed costs in Appendix D).

Interim Facilities: In advance of constructing the new outfall line, interim facilities are proposed to direct flows through existing lines as shown in the Sewer Master Plan. The proposed interim facilities would include a pumping station near the Eaton Road crossing of SR99 and a force main southeasterly to the existing Northeast Chico Assessment District trunkline at East Avenue and the Esplanade. The pumping station and force main would have a capacity of approximately 5.0 mgd (currently sized at 3.5 mgd in the City's Sewer Master Plan). Phased construction of the pump station in incremental capacities is proposed.

The gravity trunkline constructed in the interim improvement program would be sized for the ultimate design flows, and would remain in service after construction of the new gravity trunkline to the treatment plant. The total estimated cost of interim facilities is \$2,026,000. The Plan area will generate 2.48 mgd of the 5.0 mgd flowing through the off-site sewer outfall at buildout. Based upon the contribution of flows, the NCSP share of the interim facility costs is \$1,013,000 (see Appendix C for detailed costs).

Treatment: Wastewater treatment is provided at the Water Pollution Control Plant on River Road. New flows from the Plan area will add 1.05 mgd average dry weather flow to the plant.

The existing City treatment plant connection fee required at the time of the building permit will provide revenues necessary to increase the plant's capacity to accommodate the Plan area flows.

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Alternative 2: County North Chico Treatment Facility

If the City of Chico's annexation requirement proves an obstacle for sewer service to the NCSP area, or if a County treatment plant or trunk line is proposed in connection with the nitrate program, Alternative 2 is proposed as a solution.

This alternative proposes the development of a North Chico Treatment Facility owned and operated by the County through a County Service Area. A package plant would be constructed near the Plan area and west of Highway 99. The actual location of the plant site has not yet been identified and the final location may ultimately be established based upon its potential use for other County areas impacted by the groundwater nitrate plume condition.

Following the proper level of treatment, wastewater could possibly be discharged into Mud Creek, and then to the Sacramento River. The recommended use of the treated wastewater would be reclamation for agricultural irrigation. Initial discussions with the State Regional Water Quality Control Board staff have led to a recommendation for study of this alternative.

The Plan area would be served by a gravity line from Eaton Road at SR99 westerly to the new treatment plant site. The estimated cost of Alternative 2 is \$5,582,000 (see detailed cost estimate in Appendix D).

Stormwater Management

Existing Conditions: The 3,590-acre Plan area is relatively flat. Existing stormwater runoff is routed to leveed and non-leveed channels in the area within three primary drainages: Keefer Slough/Rock Creek, Mud Creek, and Sycamore Creek. Butte County currently is responsible for the maintenance of the channel levees with the U.S. Army Corps of Engineers responsible for the channel bottom. Existing drainage patterns are shown on Figure 6-2.

Keefer Slough, the primary drainage channel within the area, collects runoff from approximately 50 percent of the project area, in addition to overflows from Rock Creek during high flood stages. Keefer Slough travels in a southwesterly direction, crossing under Hicks Road and Garner Lane before being directed under State Route 99. West of SR99, Keefer Slough is practically non-existent due to agricultural land-leveling activities and eventually merges into Rock Creek; periodic out-of-bank flooding has resulted along this portion of Keefer Slough.

Flows from Rock Creek are diverted into Keefer Slough approximately 2,000 feet northeasterly of the Keefer Slough/Keefer Road Crossing which has exacerbated the periodic flooding of the SR99 crossing and adjoining homes. A new bridge has recently been completed at the Keefer Slough/Garner Lane crossing. Flow capacity of the new structure has been designed to pass drainage from the upstream Keefer Slough drainageshed plus the Rock Creek overflow.

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In addition to Keefer Slough, Mud Creek and Sycamore Creek also collect stormwater runoff from the Plan area. Levied channels along both creeks were constructed for regional flood control purposes and are limited in their ability to naturally collect and direct local stormwater runoff.

Runoff discharged into existing channels is either pumped or gravity flows via storm drains. The system is designed to prevent stormwater from passing until channel flood levels recede. Stormwater runoff from recent residential developments between Garner Lane and Mud Creek is discharged into Mud Creek via pumping through the existing levee and/or gravity storm drain system. Pumping into either Mud Creek or Sycamore Creek is regulated by the State Reclamation Board. Permits are required from the State Reclamation Board and the U.S. Army Corps of Engineers.

Flood Hazards: Two primary flood-prone areas exist within the Plan area. These areas include Keefer Slough east of SR99, south of Keefer Road and northwesterly of the levied channel of Mud Creek, and the area easterly of Mud Creek and northerly of Sycamore Creek.

Keefer Slough Watershed Area: The capacity of the Keefer Slough channel between Garner Lane and SR99 has been estimated at 525 cubic feet per second (cfs) with the existing capacity of the Highway 99 crossing rated at 600 cfs. Keefer Slough channel currently experiences uncontrolled flooding due to the Rock Creek diversion. Flows in Keefer Slough are estimated at 3,500 cfs when the anticipated overflow from Rock Creek is included. A value of 2,700 cfs has been accepted as the overflow amount from Rock Creek as determined by the "Rock Creek Flood Diversion" study prepared for Butte County Department of Public Works in May 1979. The 100-year floodplain is determined by Federal Emergency Management Administration (FEMA).

Development in this area will require mitigation of the Rock Creek diversion-related flooding issues and/or specific on-site grade modifications to remove property from the 100-year floodplain. Prior studies have indicated the possibility of routing the diverted overflow from its beginning point northeasterly of the Keefer Road/Hicks Road intersection in a southerly direction to Mud Creek.

However, the drainage study prepared for a proposed county service area in July 1982 indicated that Mud Creek was not capable of receiving the anticipated overflow from Rock Creek. Therefore, the Rock Creek overflow must either be contained in Rock Creek or improvements made to Keefer Slough to accommodate the flows.

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Possible alternatives to alleviate the flooding of Keefer Slough are as follows:

1. Flood control dam on Rock Creek upstream of the overflow point to Keefer Slough.
2. Rock Creek channel improvements, primarily levees, to contain Rock Creek flows and prevent diversion.
3. Groundwater recharge and detention basin potentially located at the abandoned gravel pit between Garner and Hicks Lane as explained below.
4. Creation of flood meadows north of Rock Creek.
5. Keefer Slough channel improvements from the Rock Creek diversion to the confluence of Keefer Slough and Rock Creek.
6. Increased flood storage through the use of off-stream detention areas and delineation of floodplains with federally subsidized flood insurance.

The limiting factor of Keefer Slough's discharge capacity is the crossing at SR99 (600 cfs) and its rated channel capacity of 525 cfs. The Rock Creek overflow of 2,700 cfs results in a required storage volume of 1,070 acre-feet.

Theoretically, this volume could be detained on-site until the flows of Keefer Slough have subsided, allowing discharge at a rate not exceeding the slough capacity. One potential detention site is the existing sand/gravel pit located east of Garner Lane and south of Keefer Slough, with an estimated capacity of 750 acre-feet, based on the May 1979 "Rock Creek Flood Diversion" study. The report indicates that by considering infiltration (groundwater recharge) the site could accommodate approximately 86 percent of the total overflow.

The remaining 14 percent would need to be achieved in one of several ways: deepen the pit to accommodate the total storage needed or acquire additional detention areas which would store the remaining 14 percent of the overflows (approximately 150 acre-feet). If the existing sand and gravel pit is used for runoff detention, the reach of Keefer Slough between the pit and Rock Creek overflow location might possibly need improvements to contain the design discharge.

Another alternative to accommodate the Rock Creek overflow would be to construct an outfall structure which would limit the overflow to 750 acre-feet and improve the existing Rock Creek channel to contain the remaining flow. A third alternative involves channel improvements to Rock Creek to contain 100 percent of the watershed's runoff, allowing no diversion to Keefer Slough.

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Sycamore Creek Watershed Area: Runoff from the southeastern portion of the Plan area is contained by the northern levy of Sycamore Creek. Outfalls are provided into Sycamore Creek at four locations and consist of corrugated metal pipes with flap gates. During high flows in Sycamore Creek, it is unlikely that any discharge from the plan area enters Sycamore Creek. Therefore, all runoff from the Plan area destined for Sycamore Creek is currently stored on-site until such time that it can gravity flow into Sycamore Creek. This storage volume has been estimated to be 425 acre-feet for the 100-year, 24-hour storm.

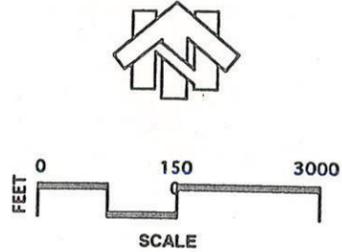
Proposed Facilities: The North Chico Specific Plan Drainage Concept is illustrated on Figure 6-3. Two different types of storm drainage systems are planned for the collection and release of stormwater into receiving channels. The Keefer Slough watershed area will use roadside ditches and drainage swales for directing runoff to Keefer Slough. Detention basins may also be included to detain runoff. Advantages of this open channel network include the reduction in peak discharge by increasing the expected travel time. As a result, the one-acre parcel designation and use of an open channel network with detention/retention does not increase the expected peak runoff from this area.

The Sycamore Creek drainage areas will incorporate the use of greenbelt drainage swales and an underground storm drainage network. Runoff will be detained in detention basins prior to discharge into Sycamore Creek. The levied channels of Sycamore Creek will likely require the pumping of storm runoff into the receiving waters. Development within this drainage area will be fully built-out with curb, gutter and sidewalk, in addition to the closed conduit system.

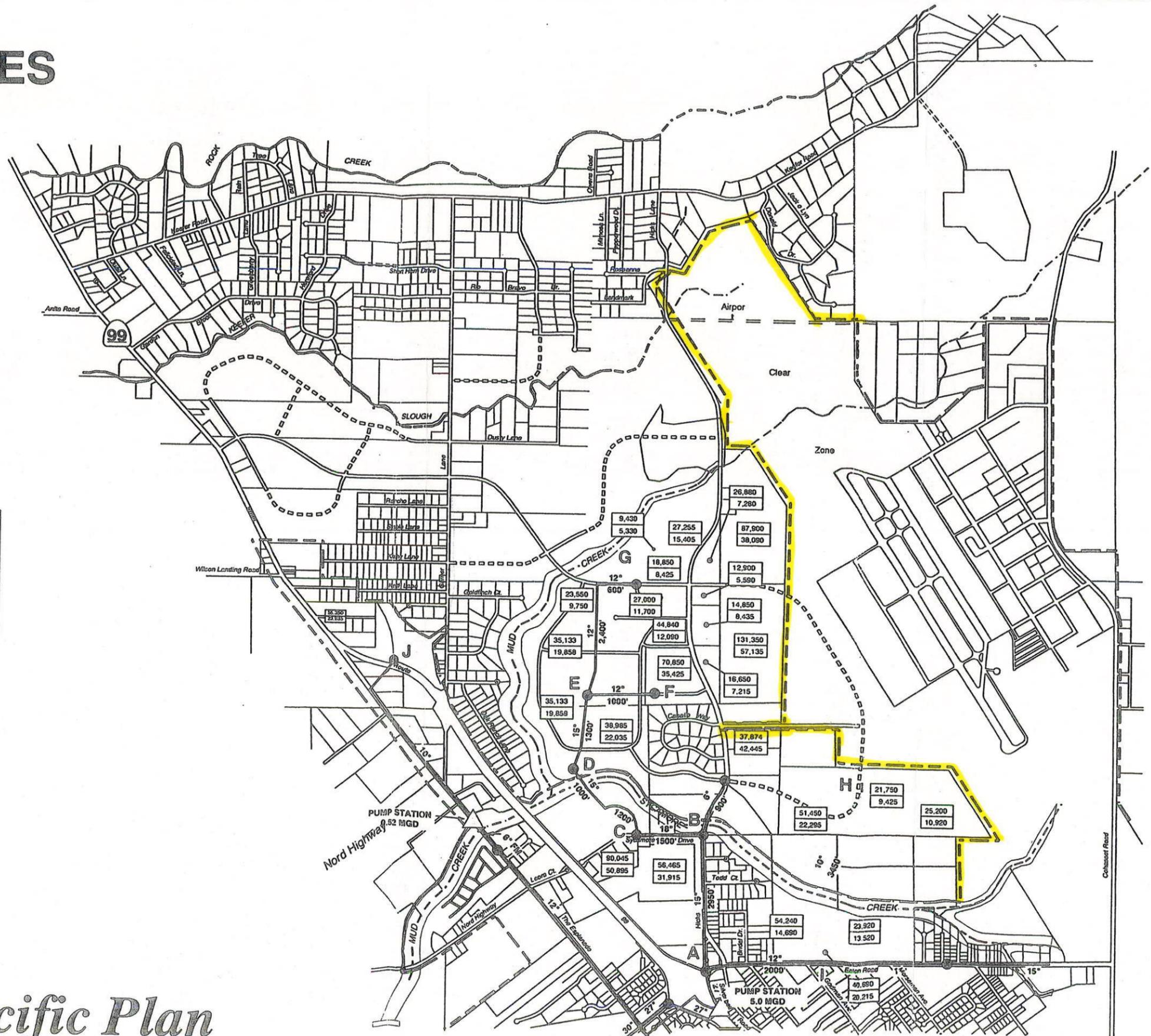
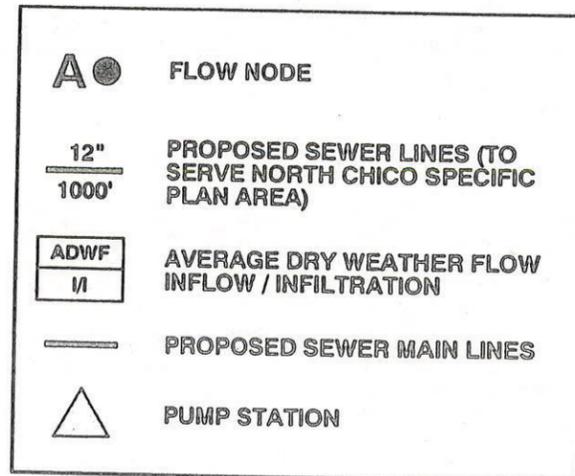
It is likely that the detention basins will be required to pump expected storm runoff into the receiving waters of Sycamore Creek after peak flows subside. The Anderson Report completed in 1979 identified approximately 3,000 cfs of excess capacity in Sycamore Creek and no capacity in Mud Creek. To pump into Sycamore Creek will require a permit from the State Department of Water Resources and the U.S. Army Corps of Engineers. To obtain a discharge/encroachment permit, the following must be accomplished:

1. Sufficient capacity of the receiving waters must be demonstrated.
2. An environmental document addressing the discharge must be completed.
3. Endorsement of the maintaining agencies; Butte County and the U.S. Army Corps of Engineers.
4. Engineering plans of the proposed drainage.

SEWER FACILITIES



LEGEND



North Chico Specific Plan

GRAVITY TRUNK LINE TO REGIONAL WATER POLLUTION CONTROL PLANT OR PACKAGED TREATMENT PLANT

FORCE MAIN TO NECSAD LINE AT EAST AVE. & ESPLANADE

Figure 6-1
SEWER FACILITIES

NORTH CHICO SPECIFIC PLAN

Schools

Existing Facilities and Projected Need: Schools in the Chico urban area are operated by the Chico Unified School District (CUSD). The District operates a total of 15 elementary schools, 2 junior high schools, and 3 high schools (includes a small continuation high school).

Existing CUSD enrollment and capacity indicate that there is currently remaining capacity for 81 junior high school students. No additional capacity remains at the elementary and senior high levels. This assessment of existing capacity assumes the use of portable/interim classroom facilities.

To meet the needs of the projected enrollment through the 2011-2012 school year, up to 10 additional schools would be needed within the District (7 elementary schools, 2 junior high schools, and 1 senior high school). If the District were to implement year-round schedules for Grades 1 to 6, the number of new elementary schools required would drop to 5.

Implementation of the North Chico Specific Plan would result in the addition of approximately 1,205 new students to the CUSD at buildout. Table 6-4 provides the number of new students by grade. As there is currently insufficient remaining capacity in the District, such an increase in the student population would result in the need for approximately 1.7 new schools (total of .9 elementary schools and approximately .4 the capacity of a junior high school and .4 the capacity of a senior high school).

**TABLE 6-4
STUDENTS GENERATED BY THE NORTH CHICO SPECIFIC PLAN**

School	Student Loading Factor	Number of New Students	Number of New Classrooms	Number of New Schools
Elementary	0.2322	651	24	0.9
Junior High	0.0989	277	11	0.4
Senior High	0.0989	277	11	0.4
Total	0.43	1,205	46	1.7

Proposed Facilities: The NCSP provides one 8.5 acre elementary school site reservation which could accommodate the elementary school student population expected to reside in the Plan area at buildout. This site is located adjacent to the planned neighborhood park and is within the Village Core (see Figure 3-2). This location will allow cost savings through joint acquisition and maintenance of recreational facilities. Additionally, such a location will provide

NORTH CHICO SPECIFIC PLAN

convenient access to other Village Core services. The junior high and senior high school students would be accommodated in either new or expanded school facilities planned for by the District.

Fire Protection

Existing Services and Facilities: The Butte County Fire Department, in cooperation with the California Department of Forestry and Fire Protection, hereafter referred to as CDF/BCFD, provides structural and wildland fire protection and other emergency services to the unincorporated areas of Butte County, as well as the cities of Biggs and Gridley.

The CDF/BCFD operates four fire stations in the Chico area which provide fire protection services to the unincorporated portions of the Chico urban area, including the NCSP area.

There are two stations north of the City of Chico (Stations 41 and 42) and two stations that are west and south of the City (Stations 43 and 44, respectively).

The North Chico and Nord Stations (#41 and #42), which are primarily responsible for serving the Plan area, have 1 and 2 engines, respectively. The Nord Station has 1 full-time firefighter during the winter and 2 firefighters in the summer, and the North Chico Station has 2 full-time firefighters year round. BCFD permanent staff is supported by two volunteer companies, the 60-member North Chico Volunteers, and the 25-member Durham Volunteers.

The Nord Station, adjacent to but without quick access to much of the plan area, has an average first response time of approximately 3 minutes. The North Chico Station, approximately 3 miles from the Plan area, has an average response time of approximately 5 minutes. A 5-minute response time is considered to be acceptable by the BCFD for urban areas (Brown, personal communication, 1992).

Planned Services and Facilities: While existing staffing, equipment, and response times are considered adequate, it is anticipated that planned growth in the area will require additional staffing, equipment, and facilities to maintain acceptable response times. The NCSP provides for a 1.7 acre fire station site reservation. A new fire station will be constructed on this site as development of the plan area requires.

On an interim basis, 1 additional full-time (24-hour) firefighter and 1 new engine will be provided for the Nord Station (#41) at 25% buildout of the Plan area, and a second additional full-time firefighter will be provided for the Nord Station at 50% buildout of the Plan area. For additional information on the phasing of these improvements, see Section 8, Implementation.

NORTH CHICO SPECIFIC PLAN

Solid Waste

Existing Services: Solid waste that is currently generated in the Plan area is collected by private companies and disposed at the only landfill within the county, the Neal Road Landfill, located south of Chico, approximately 11 miles from the Plan area. The 100-acre facility is owned by Butte County, but is privately operated. Remaining capacity is expected to last until the year 2005 (McElroy, personal communication, March 1992).

The County is in the process of updating its *Solid Waste Management Plan*, which will address issues related to planning new landfill sites. Under requirements of the State Integrated Solid Waste Management Act of 1989, the County also is currently in the process of developing a comprehensive plan to divert wastes from the landfill by 25% by 1995 and 50% by the year 2000.

A variety of efforts by numerous agencies and organizations in the County currently are underway regarding recycling and diversion of materials from the wastestream. Presently approximately 6.6 percent of waste generated in the unincorporated portion of the County is diverted from the Neal Road Landfill, and approximately 19.9 percent of the waste stream generated in the City of Chico is diverted (Solution Resources, Inc., September 1991). Except for scrap or other metal separation at the County Maintenance Yard, there are no other County-sponsored recycling programs in place. There are recycling/drop-off centers in the Chico area which recycle aluminum cans, ferrous metals, newspaper, and glass, among other materials. The City of Chico also has a permitted composting facility.

In developing its waste diversion plan, the County has identified a number of alternatives that are outlined in the *Preliminary Draft Source Reduction and Recycling Element* prepared for the County. The County is discussing the establishment of a Joint Powers Agreement with the City of Chico to provide municipal services including solid waste management services. The City of Chico now requires that each solid waste collector in the City holding a permit to collect solid waste from single-family and two-family residential customers provide these customers with a curbside recycling program. The County also is evaluating methods to reduce the waste stream of major commercial/industrial uses, and increase participation in local recycling centers.

The fact that the only County landfill is reaching capacity (in 10± years) is a regional issue that currently is being addressed through preparation of the *Solid Waste Management Plan*.

Planned Services and Facilities: Solid waste associated with Plan area buildout will continue to be collected by private companies and disposed at the Neal Road Landfill until a new landfill is constructed. Opportunities and programs to expand existing and promote new source reduction strategies are being developed under the County's *Solid Waste Management Plan*

NORTH CHICO SPECIFIC PLAN

efforts. The proximity of the Plan area to the City of Chico, where a number of recycling centers and programs are underway, offers an opportunity for coordination of potential programs, such as curbside recycling.

Development of the NCSP could also include a drop-off center and/or recycling programs, to be coordinated with other County programs. Measures to seek cooperation with County/City waste reduction efforts will be needed.

Utilities

Electricity and Natural Gas Services

Existing Facilities: Electricity and natural gas services are currently provided by Pacific Gas and Electric Company (PG&E). Existing residential development in the Plan area is served by both overhead and underground electrical facilities along Eaton Road, Garner Lane, Hicks Lane, and Keefer Road, with line extensions to serve existing subdivisions. Overhead electrical transmission lines generally are 12 kilovolts (kv). However, a 115-kv overhead transmission line exists along Eaton Road. The nearest substation to the project area is located west of SR99 on Anita Road.

Natural gas lines currently serve only the Plan area residential developments along Eaton Road.

Planned Facilities: According to PG&E, there is sufficient electrical and natural gas capacity to serve development under the NCSP. No problems in providing electrical and gas services are foreseen (Rooke, personal communication, 1992). PG&E will charge new development for extending service and trenching utility lines. Many of these fees are refunded according to regulations set forth by the California Public Utilities Commission (CPUC), Rule 15.

New development would be served through extension of existing and installation of new electrical transmission lines (12 kv). All electrical line extensions to serve residential, commercial, and industrial development would be underground, in accordance with the CPUC Rule 15, unless otherwise exempted by the CPUC. No additional distribution substations would be required to serve new Plan area development.

Natural gas services could also be extended to serve Plan area development. Lines would be extended from existing mains in the southernmost portion of the Plan area.

Telephone Services

Existing Facilities: Pacific Bell currently provides telephone services to existing residential development in the Plan area. Telephone lines currently exist along Garner Lane and Hicks Lane and are both overhead and underground.

NORTH CHICO SPECIFIC PLAN

Planned Facilities: Pacific Bell anticipates sufficient ability to serve new development in the plan area with underground or overhead facilities (Hook, personal communication, 1992). Pacific Bell will pay for the extension of services to residential development. However, extension of telephone services to non-residential development within the Plan area will be the responsibility of the developer.

Goals and Policies

Overall Goal

1. Provide public and community services which both adequately serve the community and are cost effective.

Water Policies

1. Plan for the installation of public water facilities in the Plan area south of Mud Creek, while continuing to allow for the use of well water for those developments north of Mud Creek.
2. In the area north of Mud Creek, require the use of community water system may be required parcels. The community water system shall be established in accordance with the requirements of state law.

Wastewater Disposal Policies

1. Prior to any development in the Plan area south of Mud Creek, a sewer alternative must be selected. The selected alternative shall meet the requirements of state law and Butte County.
2. In the area north of Mud Creek, community septic systems may be required parcels. Operation and maintenance of these systems shall meet the requirements of state law and standard engineering practice.
3. In the area north of Mud Creek, developments shall meet the requirements of Department of Environmental Health relative to the installation of septic systems.

Drainage Policies

1. Utilize, to the maximum extent possible, natural, or natural-appearing, stream or drainage courses for storm drainage.
2. Design stormwater facilities, including detention basins, to ensure public safety, to be visually unobtrusive and to provide, where feasible, recreational uses.

NORTH CHICO SPECIFIC PLAN

3. Development within a designated floodplain shall be required to meet County and FEMA regulations. No development shall be permitted in the floodway.
4. Utilize pollution control traps or sediment basins for storm drainage systems designed in accordance with County standards and/or requirements.
5. All drainage areas shall be maintained through amendment to the CSA, or other means acceptable to LAFCo, Butte County, and the maintaining group or agency.

Schools Policies

1. In consultation with the Chico Unified School District, plan for educational facilities in the Plan area.
2. Locate educational facilities in the Plan area adjacent to neighborhood parks in order to facilitate joint use recreation facilities.
3. Encourage CUSD and CARD to work cooperatively to provide joint use recreation facilities.
4. Impacts to school facilities within the Chico Unified School District (CUSD) shall be fully mitigated through the payment of mitigation fees in the amounts justified by CUSD prepared nexus studies, including adjustments, and adopted by the District, or through the implementation of other equivalent measures acceptable to CUSD, including those measures specified in the CUSD Board of Education Resolution No. 486-92.

The County recognizes that the new residential growth contemplated by this Specific Plan will result in significant increases in the student population of the Chico Unified School District ("CUSD"). The County also recognizes that funding limitations have severely hampered CUSD's ability to accommodate students generated by new residential development. Accordingly, the County, as a condition of approval of this Specific Plan, has required that new residential development fully mitigate its impacts to school facilities. But for this full mitigation requirement, the County would not have approved this Specific Plan.

Fire

1. Provide for the location of a future fire station and work with the appropriate fire service agencies to determine the nature and level of fire services required to serve the existing and future residents.

NORTH CHICO SPECIFIC PLAN

2. Amend the existing CSA, or provide another funding mechanism, to provide funds for continued fire services.

Implementation

Water: Water distribution mains and wells will be installed by each individual developer as needed.

Water facilities to serve new development are typically financed by the developer and can be designed and constructed by either California Water or the developer under the supervision of California Water. Once the facilities are constructed and accepted, they will be operated by, and become the property of, California Water Service. Facilities will be eligible for the reimbursement agreements normally utilized by California Water Service as approved by the California Public Utility Commission.

Water systems will be developed in accordance with State standards and California Water Service Company requirements regarding pressure, standby power, and treatment. The company requires standby power for a third of their wells, therefore, four of the wells will be equipped with generators.

In addition to financing the facilities, the developer must also reimburse California Water for the income tax they are required to pay on the dedicated facilities. The tax rate is approximately 33 percent. California Water has a developer reimbursement program which refunds the money advanced by the developer, including the income tax component, at a rate of 2.5 percent per year. Future development using the facilities financed by the developer would also be required to reimburse the developer for their proportionate share of those facilities.

Wastewater Disposal: Construction of initial interim facilities to the City of Chico system or construction of a package treatment plant would be financed through formation of a Mello-Roos Community Facilities District or Special Assessment District. The initial phase would include on-site sewer trunklines. Subsequent facilities, including treatment, will be phased according to demand and funded through a sewer fee program.

Stormwater Management: Drainage improvements will be constructed by individual developers as the need arises. Impact fees will be assessed to provide for a master drainage system and repayment for construction of oversized facilities.

Schools: A 10-acre elementary school site reservation is identified on the North Chico Specific Plan Land Use Map. Each developer within the Plan area shall pay applicable impact fees in accordance with County requirements prior to the issuance of a building permit.

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Fire: The NCSP provides for a 1.7-acre fire station site reservation as identified on the North Chico Specific Plan Land Use Map. The site will be acquired and constructed with funds from a Mello-Roos Community Facilities District. The existing County Service Area No. 87 or a new CSA will provide interim funding for 2 full-time firefighters and 1 fire engine to the Nord Station, until the new station in the Plan area is constructed. This staffing will be moved to the new station upon its completion.

7. DEVELOPMENT REGULATIONS AND DESIGN GUIDELINES

Zoning and Development Regulations

The zoning and development regulations contained herein have been established to provide criteria and requirements for the development of projects within the NCSP area. The zoning regulations are intended to be identical to the existing zones currently used in other areas of the County, except as amended by this Specific Plan. Any changes or amendments to the Butte County Zoning Ordinance will automatically change the requirements of those identified zones. Changes may include, but not be limited to, a change in the title of the zone, a change in the development requirements, a change in the uses of the zone. Additionally, it should be noted that the other requirements of the Butte County Zoning Ordinance, beyond the specific requirements of the individual zones, still apply. Zoning is one of the primary methods of this Plan's implementation.

When there is a conflict between these regulations and the applicable County ordinances, unless specifically stated herein to the contrary, the requirements of the NCSP shall govern.

Utilizing the Land Use Map, Figure 3-1 of this text, the County zoning designation for each land use designation is indicated as follows:

LAND USE	ZONING
<input type="checkbox"/> Suburban Residential (3 acre minimum)	SR-3
<input type="checkbox"/> Suburban Residential (1 acre minimum)	SR-1
<input type="checkbox"/> Low Density Residential	R-1
<input type="checkbox"/> Medium Density Residential	R-2
<input type="checkbox"/> Multi-family	R-3
<input type="checkbox"/> Light Industrial	M-1
<input type="checkbox"/> Heavy Industrial	M-2
<input type="checkbox"/> Business & Professional	BP
<input type="checkbox"/> Light Commercial	C-1
<input type="checkbox"/> General Commercial	C-2
<input type="checkbox"/> Open Space	OS
<input type="checkbox"/> Public/Quasi-Public	PQ

Table 7-1 summarizes the proposed NCSP land use designations and corresponding zone districts.

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Development Regulations

7.1 General Provisions

7.1-1 Whenever the provisions contained herein conflict with those contained in the Butte County Zoning Ordinance, the provisions of the NCSP Development Regulations as set forth herein shall take precedence.

**TABLE 7-1
LAND USE DESIGNATIONS
AND ZONING DISTRICTS**

Land Use	Acreage	General Plan Designation	NCSP Zoning	Units
Rural/Suburban Residential	248.0	Agricultural-Residential	SR-3 (Suburban Residential)	61
Suburban Residential	1,761.2	Agricultural-Residential	SR-1 (Suburban Residential)	1,291
Suburban Residential/ Open Space	138.2	Agricultural-Residential	SR-1/OS	41
Urban Residential, detached	222.2	Low Density Residential	R-1 (Residential)	889
Urban Residential/Open Space, detached	11.7	Low Density Residential	R-1 (Residential)	23
Urban Residential, attached and detached	118.4	Medium Density Residential	R-2 (Residential)	829
Multi-Family Residential	17.1	High Density Residential	R-3 (Residential)	308
Heavy Industrial/Open Space	251.2	Industrial	M-2/OS (Heavy Industrial/Open Space)	-
Light Industry	160.3	Industrial	M-1 (Light Industrial)	-
Light Industry/Open Space	115.8	Industrial	M-1/OS	-
Office	26.0	Commercial	B-P (Business & Professional)	-
Light Commercial	12.4	Commercial	C-1 (Limited Commercial)	-
General Commercial	49.6	Commercial	C-2 (General Commercial)	-
Schools, Parks, Churches	65.8	Public	PQ (Public/Quasi-Public)	-
Open Space and Resource Conservation	391.2	(various)	OS (Open Space)	-

7-2 Development Regulations and Design Guidelines

NORTH CHICO SPECIFIC PLAN

- 7.1-2 If specific development standards have not been established, or if an issue, condition, or situation arises or occurs that is not covered, provided for, or clearly understandable in the Specific Plan, then those provisions provided for by the Butte County Zoning Ordinance that are most applicable for the most similar issue, condition, or situation, as determined by the Development Services Director, or designee, shall apply.
- 7.1-3 Development may occur only after a determination, by the County staff, of consistency between the development proposal, these regulations, and any applicable policies and guidelines of the NCSP, has been made.
- 7.1-4 All construction and development within the Plan area shall comply with all provisions of applicable codes, standards, and requirements, and other Uniform Codes as adopted and updated from time to time by the County of Butte, including subdivision, building, mechanical, electrical, plumbing, fire, water, grading and excavation, design, and development regulations.
- 7.1-5 The boundaries of the NCSP area and of individual land use and zoning areas have been defined and delineated in this Plan. Adjustments resulting from final road alignments, and/or technical refinements to the development boundaries will not require a Specific Plan Amendment. The precise boundaries and acreage shall be determined by interpretation of the Development Services Director, or designee, as development plans are submitted via applications for subdivision maps, use permits, and site plan approvals.
- 7.1-6 The number of dwelling units and acreage for each land use designation is shown on the land use plan. These numbers may be revised as appropriate subject to the following:
- Any such revision shall be in accordance with established procedures intended to ensure compliance with the policies of the Specific Plan.
 - Such revisions shall take place through the subdivision map, use permit, or site plan approval process as applicable.
 - Within the Village Core boundaries, any revision to increase or decrease the number of dwelling units within a particular designation *by 10% or less* may be approved without amendment of the Plan.
 - Within the Village Core boundaries, any revision to increase or decrease the number of dwelling units within a particular designation *by more than 10%* shall be offset by a corresponding decrease in another designation so that the total number of units within the Village Core is not exceeded. Any such increase or decrease without such offset shall require amendment of the Plan.

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- Outside of the Village Core boundaries, any revision to increase the number of dwelling units within a particular designation shall be offset by a corresponding decrease in another designation so that the total number of units in the entire planning area is not exceeded.

7.1-7 Where a lawful structure or use exists at the effective date of adoption or amendment of this Specific Plan that could not be established or built under the terms of these regulations by reason of restrictions on use, area, height, yards, location on the lot, or other requirements, such structure or use may be continued so long as it remains otherwise lawful, subject to the provisions of Section 24.37 of the Butte County Zoning Ordinance.

7.2 Grading and Erosion Control

7.2-1 Prior to any development approvals within various phases of development, an overall conceptual grading and erosion control plan for each phase shall be submitted for Public Works approval. The grading plan shall be used as a guideline for subsequent detailed grading plans for individual stages of development within the phase and shall include the following:

- Techniques which shall be utilized to prevent erosion and sedimentation during and after the grading process, including but not limited to, seeding of graded areas, watering during grading activities to reduce wind erosion, and use of hay bales and filter cloth to prevent siltation of stream courses during winter months.
- Approximate time frames for grading including techniques and prohibitions during higher probability rain months.
- During construction, measures to prevent eroded soil from entering area waterways shall be implemented, including sprinkling, seeding, and revegetation of graded areas, and the placement of hay bales.

7.2-2 Grading will be permitted outside of the area of immediate development when it is consistent with an approved grading plan. Stockpile and borrow sites may be permitted within areas scheduled for future development, subject to an approved grading plan. Grading permits shall be authorized for issuance concurrent with or following the approval process of a subdivision map, use permit, or site plan approval. No site preparation shall be permitted prior to the approval of a grading plan.

7.2-3 Grading for planning areas adjoining natural open space and habitat areas shall include special consideration to ensure that development occurs in a way that will substantially maintain the natural character of the area and environmental and aesthetic values in

7-4 Development Regulations and Design Guidelines

NORTH CHICO SPECIFIC PLAN

accordance with the policies and regulations herein.

- No building site shall be approved for construction which does not have provisions for conducting surface drainage from the site to a natural drainage course, a drainage channel, or a public street. Such drainage shall be in accordance with established engineering practices and approved by the Department of Public Works.
- All exposed slopes shall be planted or otherwise protected from the effects of water runoff and erosion according to established engineering practice within 90 days after completion of grading. Planting shall be in accordance with the design and landscaping standards established for the NCSP.

7.2-4 Erosion and sediment transport shall be minimized by incorporating the following design criteria into the proposed drainage system:

- Maintain earth-lined drainage swale velocities to less than 5 ft/sec.
- Require erosion control grasses on drainage swales with velocities between 5 and 7 ft/sec.
- Require erosion control grouted rip-rap cobbles for drainage swales with velocities greater than 7 ft/sec.
- Detention facility outfall structures shall be sized in order to satisfy NPDES requirements prior to discharge into flood waters.

7.3 Drainage

7.3-1 Existing property outside the current 100-year floodplain shall design drainage facilities to limit peak runoff to flows at or below current undeveloped levels.

7.3-2 Existing property within the 100-year floodplain shall develop in accordance with County regulations and policies and FEMA regulations. In no case shall development be permitted in the floodway.

7-4 Resource Protection

7.4-1 Development adjacent to Keefer Slough shall be set back 100 feet from top of bank to preserve existing riparian vegetation and habitat, to encourage the expansion of riparian habitat, and to provide a corridor for wildlife. Proposed trails shall be aligned along the outer edge of the 100-foot buffer zone. The precise location of top of bank shall be approved in the field by the Director of Development Services or designee in conjunction with the County Public Works Director.



AREAS OF HIGH ARCHAEOLOGICAL SENSITIVITY, REQUIRING
FIELD SURVEY, REPORT AND POSSIBLE MITIGATION MEASURES
PRIOR TO DEVELOPMENT.

ARCHAEOLOGICAL AREAS

Figure 7-1

North Chico Specific Plan

NORTH CHICO SPECIFIC PLAN

7.4-2 Conduct comprehensive rare plant surveys on all currently non-cultivated land designated for development.

- If special status plant species are identified an evaluation of their significance shall be conducted in conjunction with appropriate agency biologists, based on their status and distribution in the area. Suitable mitigation will be identified for those species whose presence on-site is considered significant. Suitable mitigation could include avoidance (including the protection of habitat and watershed from alteration and disturbance) or establishment of on-site/off-site transplantation site(s). In this event, a mitigation plan to compensate for the removal of rare plant species shall be developed, in consultation with appropriate agency biologists, to ensure no net loss of species.

7.4-3 Conduct pre-construction surveys for properties in the vicinity of Keefer Slough between Garner Lane and Hicks Lane to determine if nesting/breeding activities of the black-shouldered kite are occurring. Delay construction activities within 300' of nest until the young have been fledged.

7.4-4 An archaeological survey and written report shall be prepared by a qualified archaeologist addressing proposed development within areas of high sensitivity as shown on Figure 7-1.

7.4-5 If, during any phase of project construction, archaeological resources or human remains are discovered, work shall be immediately halted within 150 feet of the find. The Butte County Development Services Department shall be notified, and work shall not be resumed until the find has been evaluated by a qualified professional archaeologist as described in Regulation 7.4-4 above. If the find is determined to be significant, appropriate mitigation measures shall be formulated and implemented prior to resumption of work.

7.5 Wetlands Mitigation

Development of any area which directly or indirectly impacts jurisdictional wetlands shall be subject to the following procedure for wetlands delineation and mitigation:

7.5-1 *Conduct formal wetland delineations.* Delineations shall be carried out on all areas of potential concern that are proposed for development or infrastructure. Delineations are the responsibility of individual landowners and shall be carried out according to the methodology recognized by the U. S. Army Corps of Engineers (Corps), presently the 1987 Corps manual. Delineation study areas will include all areas potentially of concern under applicable wetland regulations.

7-6 Development Regulations and Design Guidelines

NORTH CHICO SPECIFIC PLAN

This includes not only the sites of direct alteration (e.g. discharge of dredged or fill material), but also all upstream and downstream wetland areas that may be subject to hydrologic alterations resulting from fills, excavation, or drainage improvements.

Figure 5-2 identifies those areas that would require a formal delineation for development to proceed.

7.5-2 Obtain Corps permits. Wetland delineations shall be reviewed and approved by the Corps, and the required processes completed resulting in the issuance of nationwide, regional or individual permits.

7.5-3 Compensate for unavoidable wetland fills. Compensation will be in accordance with Corps regulations. Compensation for fills or alterations of wetland habitat shall ensure that an equal or greater acreage of wetlands, of equal or greater functions and values, will be created and shall include compensation for temporary habitat losses. These and other considerations usually require that proposed compensation exceed the acreage of wetland affected by a ratio of 2 to 1. Wetland creation or enhancement shall be conducted according to compensation plans approved by the Corps. Each proposed project will be responsible for adherence to Section 404(b)(1) guidelines, and for providing compensation for wetland fills either independently or in conjunction with other projects, subject to Corps approval.

7.5-4 If elements of project design or of a Plan amendment would eliminate or render unsuitable any designated preservation/mitigation area, the documentation for that project or amendment must include designation of another mitigation area of equal or greater size and suitability.

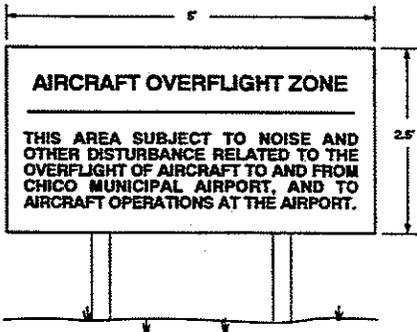
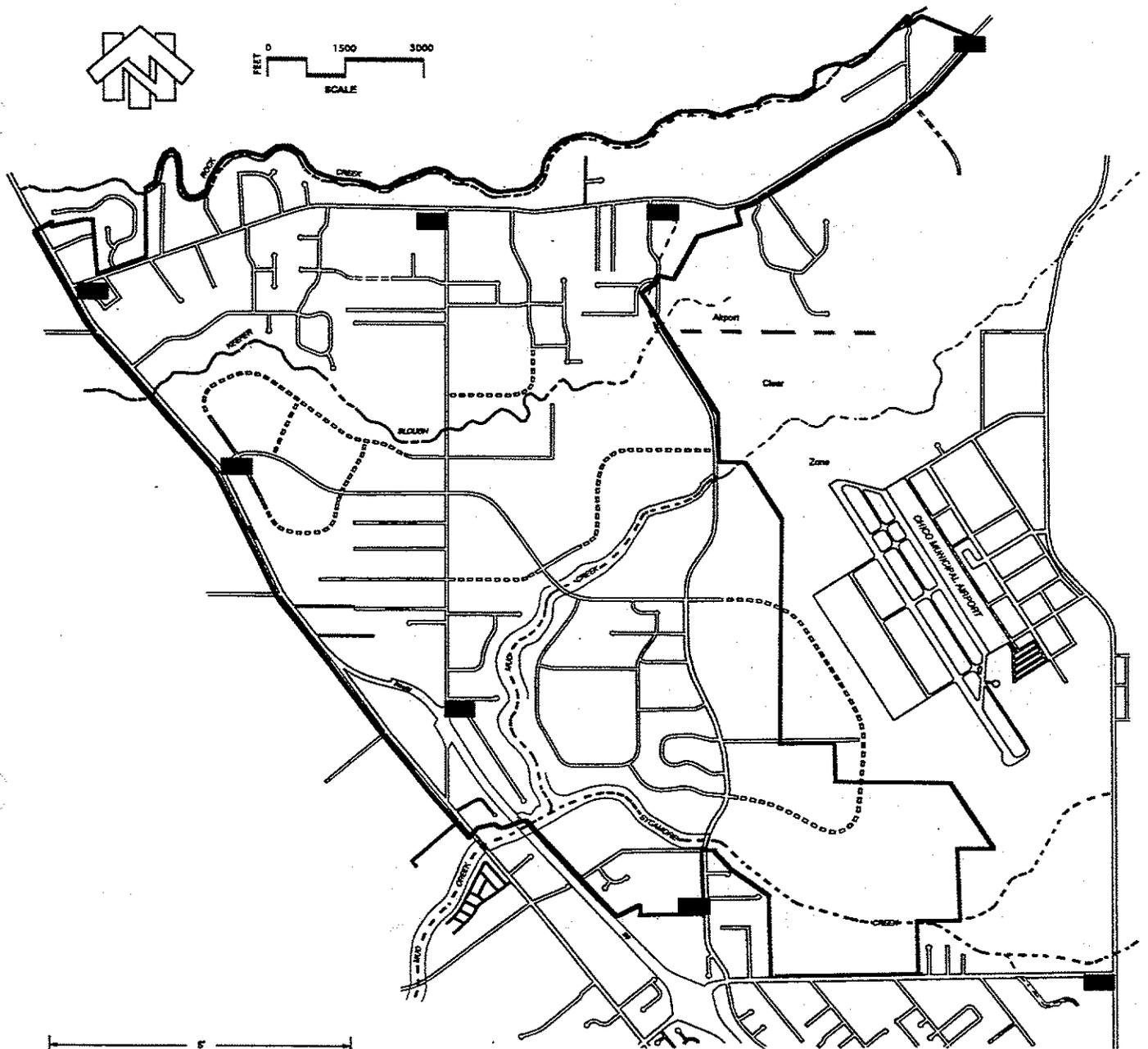
- **Preserve and protect existing or created wetlands.** During nearby construction, establish 50-foot buffers around wetlands (as measured from the outer edge of the pool's zone of influence) by construction of a barrier to prevent damage. Prohibit alteration of drainage into or out of a wetland. Prohibit artificial drainage or deposition into a wetland or its drainage without proper engineering design and necessary permits. Prohibit placement of materials or substances into a wetland or its drainage.

7.6 Protection of Chico Municipal Airport Operations

The proximity of the Plan area to the Chico Municipal Airport raises compatibility issues for development which are addressed with the following regulations:

7.6-1 Avigation easements shall be required for all lands within the Plan area.

7.6-2 Noise attenuation features shall be incorporated into new construction.



■ APPROXIMATE LOCATION OF "OVERFLIGHT ZONE" SIGNS

OVERFLIGHT ZONE SIGNAGE

Figure 7-2

North Chico Specific Plan

NORTH CHICO SPECIFIC PLAN

- 7.6-3 Enhanced disclosure measures shall be developed and implemented to alert prospective home buyers and rental tenants as to the proximity of the Chico Municipal Airport, the existence of aviation easements, the existing and projected future overflight and noise levels, and such related issues as are appropriate to fully inform such prospective home buyer or rental tenant. Enhanced disclosure may be modeled on Butte County Code, Chapter 35 Protection of Agricultural Land.
- 7.6-4 "Overflight Zone" road signage shall be installed at key access points into the Plan area, including Eaton Road, Garner Lane, Hicks Lane, New Arterial Road and Keefer Lane. Such signage shall be of such materials, size and design to be visible and readable from a moving vehicle. Figure 7-2 illustrates the concept.
- 7.6-5 The New Arterial Road and collector streets east of Garner Lane shall be designated with aviation-related names as set forth in Butte County Code Chapter 32.

7.8 Principal Land Use Districts

The following section summarizes the principal underlying land use districts within the NCSP area. The existing Butte County Zoning Ordinance has been followed closely and modified, where necessary, to ensure that NCSP goals and policies are achieved while facilitating administration by county staff.

Definitions and Conditions

Terms used in this document shall have the same definitions as provided for in the Butte County Code, unless otherwise defined herein. All regulations, design criteria, requirements, and similar details not set forth herein shall be those as contained in appropriate sections of the Butte County Code.

Residential Development Regulations

Within the Plan area there are two basic residential categories: Suburban Residential and Residential. The standards contained herein are consistent with the County zoning standards although the designations are modified or "customized" for the NCSP. The Suburban Residential designation results in two districts, SR-3 and SR-1. The Residential designation has three zone districts — R-1 (Low Density Residential), R-2 (Medium Density Residential), and R-3 (High Density Residential). Table 7-2 summarizes basic development regulations of each district.

**TABLE 7-2
SUMMARY OF ZONE DISTRICT DEVELOPMENT STANDARDS (Option B)**

Design Standard	Zoning Districts													
	SR-J	SR-1	R-1	R-2	R-3	BP	C-1	C-2	M-2	M-1	P-Q	PD Res	PD Non Res	
Lot Configuration														
Minimum Lot Size	Interior	3 ac.	1 ac.	8000 sf	4000 sf	6500 sf	6500 sf	2000 sf	None ³	None ³	None ³	None ³	6,500 ²	None ^{1,2}
	Corner	3 ac.	1 ac.	8000 sf	4000 sf	7000 sf	7000 sf	2000 sf	None ³	None ³	None ³	None ³	6,500 ²	None ^{1,2}
Minimum Lot Width ¹	Interior	130 ft	130 ft	65 ft	45 ft	50 ft	50 ft	25 ft	25 ft	None ³	None ³	None ³	2	None ^{1,2}
Maximum Building Coverage ¹²	-	10%	15%	50%	55%	60%	60%	60%	60%	70%	60%	50%	11	70%
Minimum Landscaped Open Space ²	-	None	None	40%	30%	25%	25%	5%	5%	5%	5%	30%	10	10%
Open Space	-	-	-	-	-	-	-	-	-	-	-	-	25%	25%
Site Development Requirements	-	-	-	-	-	-	-	-	-	-	-	-	10	10
Parking														
Required Off-Street Spaces	-	2/du	2/du	2/du	2/du	2/du	1/300 sf	1/200 sf	Code ⁴	Code ⁴	Code ⁴	Code ⁴	2/du	Code ⁴
Surfacing Material	-	None	None	Pcc/ac ⁵										
Building Setbacks & Configurations (In feet)														
Front	-	50 ⁶	50 ⁶	20	20	20	20	None ⁷	50 ⁶	None ⁷				
Rear	-	20	20	15	15	10	5	None ⁷	15	None ⁷				
Side	-	5	5	5	5	5	5	None ⁷	5	None ⁷				
Minimum Distance Between the Buildings	-	10	10	10	10	10	10	None						
Maximum Building Height	-	35	35	35	35	40	50	50	50	50	50	35	35	50

Footnotes:

- | | |
|---|---|
| <p>¹ Measured from property line on edge of right-of-way.</p> <p>² Percentage of total lot area.</p> <p>³ Served by public sewer, or must meet onsite disposal standards.</p> <p>⁴ Requirements vary by specific use within zone and shall be as set forth in the Bulle County Code, Section 24-35.</p> <p>⁵ Portland cementitious concrete (concrete) or asphaltic concrete.</p> <p>⁶ Measured from centerline of street, or 20' from property line, whichever is greater.</p> <p>⁷ Front setback shall be equal to adjacent use for a minimum distance of 50' where abutting an "R" zone.</p> <p>⁸ 5' required if lot abuts an "R" district; if lot is on corner adjacent to an "R" district, side yard shall be 15 feet.</p> | <p>⁹ The Planned Development (PD) Combining Zone allows for a variety of lot sizes to accommodate the designated yield.</p> <p>¹⁰ Development is to conform to the Site Development Requirements as specified by the North Clivco Specific Plan under the Planned Development (PD) Combining Zone.</p> <p>¹¹ in accordance with Table 7-2 (a)</p> <p>¹² Maximum Building Coverage is a total of all structures including detached barns, garages, gazebos, etc.</p> |
|---|---|

Table above reflects amendments approved by the Board of Supervisors on August 17, 2004.

Table 7-2 (a)
Max. Building Coverage Limitations for the PD
Combining Zone

NOTE: Maximum Building Coverage is a total of all structures including detached barns garages gazebos etc

Parcel Size	Max Building Coverage
>1-acre	6,530 sq ft
> ¼-acre - ≤1-acre	6,530 sq.ft
> ½-acre - ≤¾-acre	6,530 sq ft
>1/3-acre - ≤½-acre	5,800 sq ft
>1/4-acre - ≤1/3-acre	4,900 sq ft
6,000 - ≤4-acre	3,250 sq ft

Table above reflects amendments approved by the Board of Supervisors on August 17, 2004.

NORTH CHICO SPECIFIC PLAN

SR-3 (Suburban Residential)

1. Site Requirements

- Lot Area Required: 3 acre minimum lot size per dwelling unit, including corner lots.
- Lot Width Required: 130 feet as measured from property line at edge of right-of-way, including corner lots.
- Front Yard Required: 50 feet centerline of adjacent streets, or 20 feet from property line, whichever is greater.
- Rear Yard Required: 20 feet
- Side Yard Required: 5 feet
- Maximum Building Height: 35 feet
- Maximum Building Coverage: 10% of lot area.
- Minimum Landscaped Open Space: None
- Off-street Parking Required: 2 spaces per dwelling unit, no surfacing material required.

2. Uses Permitted

- (1) One single-family dwelling per parcel, not including tents or trailers;
- (2) Accessory buildings pertinent to the permitted uses;
- (3) Agricultural uses, including the keeping of animals, subject to the animal maintenance requirements of Sections 24-35.1 through 24-35.3 of the Zoning Code.

3. Uses Requiring Use Permits

The following uses are permitted subject to securing a use permit in each case:

- (a) Public and quasi-public uses including churches, firehouses, and public utility buildings;
- (b) Sales tract office.

NORTH CHICO SPECIFIC PLAN

SR-1 (Suburban Residential)

1. Site Requirements

- Lot Area Required: 1 acre minimum lot size per dwelling unit, including corner lots.
- Lot Width Required: 130 feet as measured from property line at edge of right-of-way, including corner lots.
- Front Yard Required: 50 feet centerline of adjacent streets, or 20 feet from property line, whichever is greater.
- Rear Yard Required: 20 feet.
- Side Yard Required: 5 feet.
- Maximum Building Height: 35 feet.
- Maximum Building Coverage: 15% of lot area.
- Minimum Landscaped Open Space: None
- Off-street Parking Required: 2 spaces per dwelling unit, no surfacing material required.

2. Uses Permitted

- (1) One single-family dwelling per parcel, not including tents or trailers;
- (2) Accessory buildings pertinent to the permitted uses;
- (3) Agricultural uses, including the keeping of animals, subject to the animal maintenance requirements of Sections 24-35.1 through 24-35.3 of the Zoning Code.

3. Uses Requiring Use Permits

The following uses are permitted subject to securing a use permit in each case:

- (a) Public and quasi-public uses including churches, firehouses, and public utility buildings;
- (b) Sales tract office.

NORTH CHICO SPECIFIC PLAN

R-1 (Low Density Residential)

1. Site Requirements

- Lot Area Required: 8,000 square feet, including corner lots.
- Lot Width Required: 65 feet interior lots, 75 feet corner lots.
- Front Yard Required: 20 feet.
- Rear Yard Required: 15 feet, except 5 feet for accessory building when abutting an alley
- Side Yard Required: 5 feet, except street side of corner lot - 10 feet.
- Maximum Building Height: 35 feet
- Maximum Building Coverage: 50% of lot area.
- Minimum Landscaped Open Space: 40% of lot area.
- Off-street Parking Required: 2 spaces per dwelling unit, concrete or asphalt surface.

2. Uses Permitted

- (1) One single-family dwelling per parcel;
- (2) Accessory buildings

3. Uses Requiring Use Permits

The following uses are permitted subject to securing a use permit in each case:

- (a) Rest homes, convalescent homes and sanitariums for more than six (6) residents
- (b) Licensed family, foster or group homes in conjunction with seven (7) or more children and day care facilities for over twelve (12) children
- (c) Social halls, lodges, fraternal organizations and clubs operated by recognized non-profit organizations
- (d) Sales tract offices

NORTH CHICO SPECIFIC PLAN

R-2 (Medium Density Residential)

1. Site Requirements

- Lot Area Required: 4,000 square feet, including corner lots.
- Lot Width Required: 45 feet interior, 50 feet corner
- Front Yard Required: 20 feet
- Rear Yard Required: 15 feet, except 5 feet for accessory building when abutting an alley
- Side Yard Required: 5 feet, except street side of corner lot - 10 feet. May be no set back if in connection with an approved planned unit development in which zero-lot-line planning is utilized.
- Maximum Building Coverage: 55% of lot area.
- Minimum Landscaped Open Space: 30% of lot area.
- Maximum Building Height: 35 feet
- Minimum Landscaped Open Space: 30% of lot area
- Off-street Parking Required: 2 spaces per dwelling unit, concrete or asphalt surface.

2. Uses Permitted

- (1) One single-family dwelling per parcel;
- (2) One duplex or zero lot line single-family dwelling (halfplex per parcel).
- (3) Accessory buildings

3. Uses Requiring Use Permits

The following uses are permitted subject to securing a use permit in each case:

- (a) Rest homes, convalescent homes and sanitariums for more than six (6) residents
- (b) Licensed family, foster or group homes in conjunction with seven (7) or more children and day care facilities for over twelve (12) children
- (c) Social halls, lodges, fraternal organizations and clubs operated by recognized non-profit organizations
- (d) Sales tract offices

R-3 (High Density Residential)

1. Site Requirements

- Lot Area Required: 6,500 square feet, 7,000 square feet corner lots.
- Lot Width Required: 50 feet interior, 65 feet corner
- Front Yard Required: 20 feet
- Rear Yard Required: 10 feet, except 5 feet for accessory building when abutting an alley
- Side Yard Required: 5 feet, except street side of corner lot - 15 feet.
- Maximum Building Coverage: 60% of lot area.
- Minimum Landscaped Open Space: 25% of lot area.
- Maximum Building Height: 40 feet
- Off-street Parking Required: 2 spaces per dwelling unit, concrete or asphalt surface.

2. Uses Permitted

- (1) Multiple family dwellings, condominiums, and apartments
- (2) Duplex or two-family dwellings
- (3) Accessory buildings

3. Uses Requiring Use Permits

The following uses are permitted subject to securing a use permit in each case:

- (a) Churches
- (b) Rest homes, convalescent homes and sanitariums for more than six (6) residents
- (c) Licensed family, foster or group homes in conjunction with seven (7) or more children and day care facilities for over twelve (12) children

NORTH CHICO SPECIFIC PLAN

BP (Business & Professional Office)

1. Site Requirements

- Minimum Lot Size: 6,500 square feet interior, 7,000 square foot corner lot.
- Lot Width Required: 50 feet interior, including corner.
- Front Yard Required: 20 feet.
- Rear Yard Required: 5 feet.
- Side Yard Required: 5 feet.
- Maximum Building Coverage: 60%
- Maximum Building Height: 50 feet or 4 stories.
- Minimum Landscaped Open Space: 25% of lot area, including parking lot interior and perimeter landscaping.
- Off-street Parking Required: 1 space per 300 square feet, concrete or asphalt.

2. Uses Permitted

- (1) General office
- (2) Professional office and clinics
- (3) Corporate and regional headquarters
- (4) Banking, insurance, and other financial operations
- (5) Computer programming, data processing, and other software services

NORTH CHICO SPECIFIC PLAN

C-1 (Light Commercial)

1. Site Requirements

- Minimum Lot Size: 2,000 square feet, including corner lot.
- Minimum Lot Width: 25 feet, including corner lot.
- Front Yard Required: None, except where the front setback abuts an R district, in which case the front yard shall be equal to the adjacent use for a distance of not less than 50 feet.
- Rear Yard Required: None, except where the rear yard abuts an R district, in which case the rear yard shall be not less than five (5) feet.
- Side Yard Required: None, except when the side of a lot abuts upon the side of a lot in an R district, in which case the abutting side yard shall not be less than five (5) feet; and except where the side yard on the street side of a corner lot abuts on an R district, in which case the side yard on the street side shall be one-half the front yard required in such R districts.
- Maximum Lot Coverage: 60%
- Minimum Landscaped Open Space: 5% of lot area, including parking lot interior and perimeter landscaping.
- Maximum Building Height: 50 feet
- Parking Requirements: As required by Butte County Code section 24-35.

2. Uses Permitted

- (1) Retail stores and shops of light commercial character and conducted within a building, including appliance stores, banks, and laundrettes, dress shops, drugstores, food stores, furniture shops, studios and tailor shops, public utility and other uses which are of similar character to those enumerated and which will not be detrimental or obnoxious to the neighborhood in which they are to be located;

3. Uses Requiring Use Permits

The following uses are permitted subject to securing a use permit in each case:

- (a) Small animal hospitals, dance academies, pet shops, refreshment stands, service stations, drive-in restaurants and other uses which are of similar character;
- (b) Recycling facilities as per section 24-68.

NORTH CHICO SPECIFIC PLAN

C-2 (General Commercial)

1. Site Requirements

- Lot Area Required: None
- Lot Width Required: 25 feet, including corner lots.
- Front Yard Required: None
- Rear Yard Required: None, except where the rear yard abuts an R district, in which case the rear yard shall be not less than five (5) feet.
- Side Yard Required: None, except when the side of a lot abuts upon the side of an R district lot, in which case the abutting side yard shall not be less than five feet; and except where the side yard on the street side of a corner lot abuts on an R district, in which case the side yard on the street side shall be one-half the front yard required in the R district.
- Maximum Building Coverage: 60%
- Minimum Landscaped Open Space: 5% of lot area, including parking lot interior and perimeter landscaping.
- Maximum Building Height: 50 feet
- Parking Requirements: Off-street parking shall be provided at the rate as required by Butte County Code section 24-35.

2. Uses Permitted

General commercial uses, including art shops, aviaries, bars and cocktail lounges, billiard parlors and pool halls, bowling alleys, building material (retail), cleaning and pressing establishments, dance halls, interior decorating shops, employment agencies, governmental legislative buildings, gymnasiums, public commercial or physical cultural studios, hospitals, hotels and motels, laboratories and X-ray facilities, equipment rentals, pet shops, used car lots, repair garages, auto car washes, plumbing shops, cabinet shops, sign manufacturing shops, drive-in restaurants and other retail establishments when interpreted as similar.

3. Uses Requiring Use Permits

The following uses are permitted subject to securing a use permit in each case:

- (a) Welding shops;
-

NORTH CHICO SPECIFIC PLAN

- (b) Manufacturing of clothing, handicraft products, printing, lithographing and other light manufacturing or industrial uses of similar character;
- (c) Public or quasi-public uses, including churches;
- (d) Service stations, kennels and small animal hospitals and mortuaries;
- (e) Recycling facilities as per section 24-68 of the Zoning Code.

NORTH CHICO SPECIFIC PLAN

M-1 (Light Industrial)

1. Site Requirements

- Lot Area Required: None
- Lot Width Required: None
- Front Yard Required: 10 feet minimum, 20 feet average.
- Rear Yard Required: None, except where the rear yard abuts an R district, in which case the rear yard shall be not less than five (5) feet.
- Side Yard Required: None, except when the side of a lot abuts upon the side of a lot in an R district, in which case the abutting side yard shall not be less than five (5) feet; and except where the side yard on the street side of a corner lot abuts on an R district, in which case the side yard on the street side shall be one-half the front yard required in such R districts.
- Maximum Building Coverage: 60%
- Minimum Landscaped Open Space: 5% of lot area, including parking lot interior and perimeter landscaping.
- Maximum Building Height: 50 feet
- Parking Requirements: Off-street parking shall be provided at the rate as required by Butte County Code section 24-35.

2. Uses Permitted

- (1) Wholesale and storage warehouses;
- (2) The assembly and storage of goods, materials, liquids and equipment (except the storage of inflammable matter or explosives or materials which create dust, odors or fumes);
- (3) The manufacturing, processing, fabricating, assembling, refining, repairing, packaging, and treatment of goods, materials and products by power (oil, gas or electric), including, by way of example and illustration, but not limited to, the manufacturing, fabrication and assembling of bathroom shower and patio partitions, enclosures and doors, windows and store fronts of aluminum, plastic and fiberglass, or other suitable material, and the processing, finishing, polishing and anodizing of aluminum extrusions and castings, assaying, broom and brush manufacturing, die casting, draying, freighting or trucking yards or terminals, heavy equipment rental or sale, heating and ventilating service shops, jewelry manufacturing, lumberyards, packaging plants, public utility service yards, truck repairing and overhauling

NORTH CHICO SPECIFIC PLAN

and welding shops;

- (4) Dyeing and rug cleaning plants, veterinary hospitals and animal shelters and boarding kennels, cabinet and woodworking shops, construction and material yards;

3. Uses Requiring Use Permits

The following uses are permitted subject to securing a use permit in each case:

- (a) Storage of inflammables;
- (b) Manufacturing of acids, explosives, fertilizer, glue, gypsum, lime, plaster of paris, pulp and paper, beet sugar, crushed rock, sand and gravel, cement, concrete and/or asphalt batching plants, concrete and clay products;
- (c) Industrial uses which might be objectionable by reason of emission of noise, offensive odor, smoke, dust, bright light, vibration or involving the handling of explosives or dangerous materials;
- (d) Recycling facilities as per section 24-68.

NORTH CHICO SPECIFIC PLAN

P-Q (Public, Quasi-Public)

1. Site Requirements

- Minimum Lot Size: None
- Minimum Lot Width: None
- Minimum Front Yard: 20 feet
- Minimum Side Yard: 5 feet, except where the side yard on the street side of a corner lot abuts on an R district, in which case the side yard on the street side shall be one-half the corresponding front yard required in such R district.
- Maximum Building Coverage: 60%
- Minimum Landscaped Open Space: 5% of lot area, including parking lot interior and perimeter landscaping.
- Maximum Building Height: 50 feet
- Parking Requirements: Off-street parking shall be provided at the rate as required by Butte County Code section 24-35.

2. Uses Permitted

- (1) Public schools; parks, playgrounds, publicly owned buildings, land, and recreational areas.
- (2) Churches, and the recreational, educational, religious, accessory residential and similar uses incidental to a cohesive campus of related activities.

OS (Open Space)

Permitted Uses

- (1) Biking and hiking trails
- (2) Drainage improvements
- (3) Walls and fences
- (4) Public parks (recreation centers and facilities)
- (5) Wetland preserves, reference to section 7.5 Wetlands Mitigation.
- (6) Private parks (active and passive)
- (7) Any other uses deemed compatible by the Director of Development Services or designee.

NORTH CHICO SPECIFIC PLAN

ROAD DESIGN STANDARDS

Road Cross Sections

Arterial streets include Garner Lane, Hicks Lane re-alignment and widening (from Eaton to the village core), Keefer Road (from Hicks Lane westerly), and the new road extending from Hicks Lane to State Route 99 (termed "New Arterial Road" or "New Arterial" herein). Arterial street cross sections vary in accordance with function, projected traffic load, and surrounding land uses. Both two-lane and four-lane arterials are specified. Class I bike paths and Class II bike lanes will be built within the right-of-way where shown in Figure 4-2 Paths & Trails. Drainage swales may occur on one or both sides of the streets either within the right-of-way or in an adjacent drainage corridor. Lots fronting on arterials are allowed only in situations where there is no practical alternative. On-street parking is not allowed except in the heart of the Village Core.

Collector streets provide access from local residential streets to arterial streets and also serve as residential streets. Within the residential (R) zoning districts, collector streets will include curb, gutter and sidewalk, together with planting strips and street trees. Collector streets in the suburban-residential (SR) zoning districts retain the rural character of the area with open shoulders and drainage swales.

Local streets directly serve residential uses and, similarly, are designed with or without urban characteristics depending on location and land use. Local streets are intended to encourage slow speeds and pedestrian activity.

Alleys are specified in the residential areas where front driveways and garages would dominate the street, and where safety is enhanced by reducing turning movements. Rear parking provided by alleys allows for a much more attractive street.

Table 7-3 presents a summarized version of street characteristics, and Figure 7-3 illustrates each street type in cross-section.

**TABLE 7-3
SUMMARY OF STREET DESIGN STANDARDS**

Street Type	Name - Location - Criteria	R.O.W. Width	# of Travel Lanes	Pvmt. Width ⁽¹⁾	Curb & Gutter	Side-walk	Bike Path	On-Street Parking	Street Lights	Design Stds. ⁽²⁾
Arterial Streets										
Type A-II	New Arterial — Eaton to Village Core	64'	4	64'	Yes	Yes ⁽³⁾	Cl. 2	No	Yes	RS-2(A)
Type A-III	New Arterial — Hicks to Mud Creek	60'	2	40'	Yes	Yes ⁽³⁾	Cl. 2	No	Yes	RS-2(A)
Type A-IV	New Arterial — Mud Creek to SR99; Keefer Road; Garner — Keefer to SR99; Hicks — Eaton to Caballo.	60'	2	36'	No	No	Cl. 1 & 2 ⁽⁴⁾	No	No	RS-3(A)
Collector Streets										
Type C-I	Village Core loop and (3) streets connecting it to Hicks; Hicks — Mud Creek to Caballo; New connector btwn Hicks and New Arterial; Sycamore Lane	60'	2	40'	Yes	Yes ⁽³⁾	No	Yes	Yes	RS-2(A)
Type C-II	New street connecting Garner and Hicks; Hicks — Keefer to Mud Creek.	60'	2	32'	No	No	Cl. 2 ⁽⁴⁾	No	No	RS-3(A)
Local Streets										
Type L-I	All local streets in "M" zones.	60'	2	40'	Yes	Yes ⁽³⁾	No	No	Yes	RS-1(A)
Type L-II	All local streets in "R" zones not otherwise described.	60'	2	40'	Yes	Yes ⁽³⁾	No	Yes	Yes	RS-2(B)
Type L-III	All local streets in "SR" zones not otherwise described.	50'	2	28'	No	No	No	Yes	No	RS-2(C)
Type L-IV	Cul-de-sac streets in "R" zones .	50'	2	32'	Yes	Yes ⁽³⁾	No	Yes	Yes	RS-2(C)
Type L-V	Cul-de-sac streets and streets in "SR" zones serving fewer than 12 lots.	50'	2	24'	No	No	No	No	No	RS-3(C)
Type L-VI	New one-way loop street located between Garner and SR99.	30'	1	18'	No	No	No	One Side	No	RS-3(C)
Type L-VII	Alleys within R-1 and R-2 zones .	22'	1	18'	No	No	No	No	No	RS-9-LD(I)

¹ Pavement width includes gutter pan where applicable, but does not include aggregate base shoulder.

² Refers to design details as set forth in standard plans used by the Butte County Department Of Public Works.

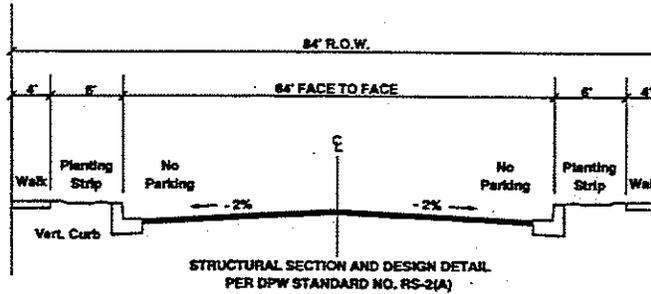
³ Sidewalk to be separated from curb by planter strip, see cross-section for dimensions.

⁴ Bike paths and lanes only where shown in Figure 4-2.

STREET CROSS SECTIONS

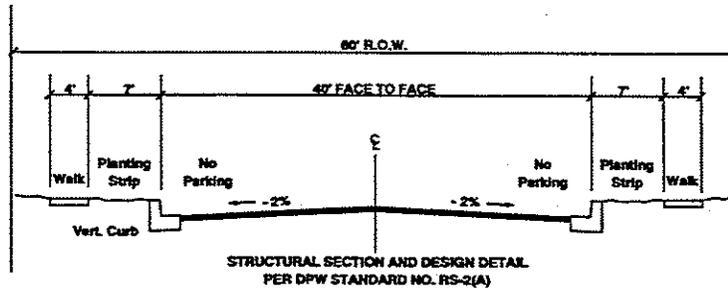
Figure 7-3a

ARTERIAL TYPE A-II



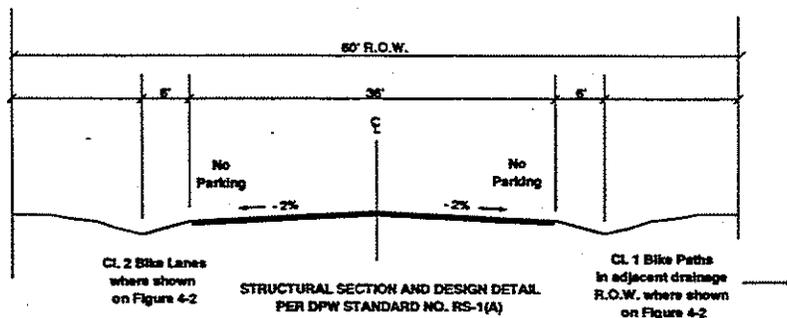
4-LANE ARTERIAL HICKS LANE REALIGNMENT & WIDENING FROM EATON TO VILLAGE CORE

ARTERIAL TYPE A-III



NEW ARTERIAL AT VILLAGE CORE BETWEEN HICKS LANE WESTERLY TO MUD CREEK

ARTERIAL TYPE A-IV



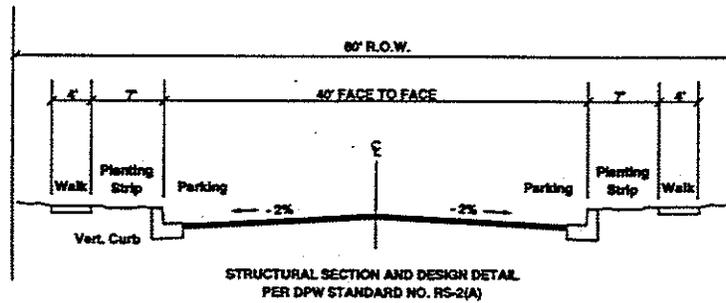
NEW ARTERIAL BETWEEN MUD CREEK AND SR99; KEEFER BETWEEN HICKS LANE AND SR99; HICKS LANE FROM MUD CREEK TO KEEFER RD.; GARNER LANE BETWEEN KEEFER AND SR99

North Chico Specific Plan

STREET CROSS SECTIONS

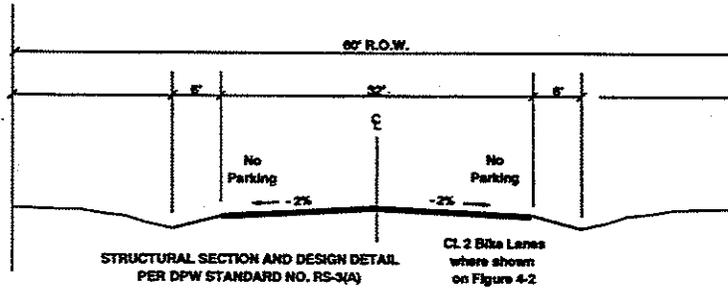
Figure 7-3b

COLLECTOR TYPE C-I



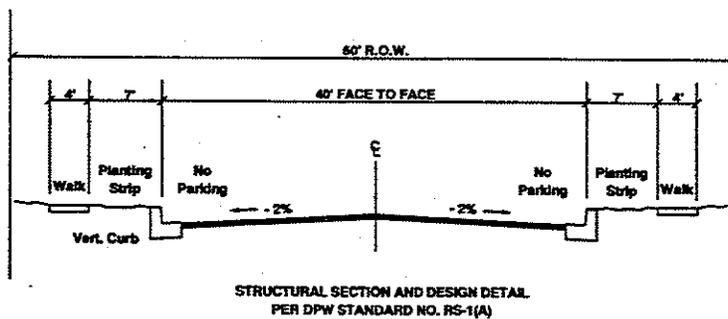
VILLAGE CORE LOOP ROAD AND CONNECTING STREETS TO HICKS LANE;
NEW CONNECTING ROAD BETWEEN HICKS AND NEW ARTERIAL;
SYCAMORE LANE.

COLLECTOR TYPE C-II



HICKS LANE BETWEEN KEEFER ROAD AND MUD CREEK.

LOCAL TYPE L-I



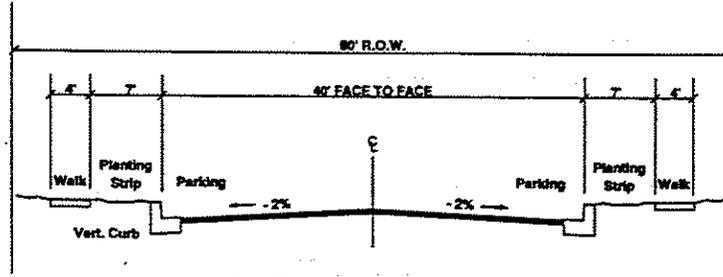
ALL LOCAL STREETS IN M-1 (LIGHT INDUSTRIAL)
AND M-2 (HEAVY INDUSTRIAL ZONES)

North Chico Specific Plan

STREET CROSS SECTIONS

Figure 7-3c

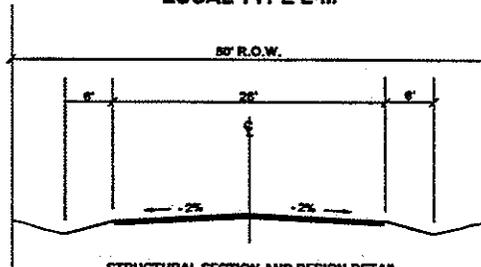
LOCAL TYPE L-II



STRUCTURAL SECTION AND DESIGN DETAIL
PER DPW STANDARD NO. RS-2(B)

ALL LOCAL STREETS IN R-1, R-2, AND R-3 ZONES
UNLESS OTHERWISE DESCRIBED.

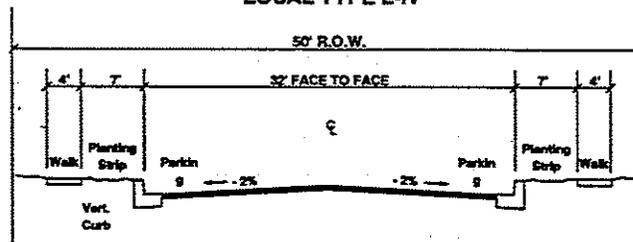
LOCAL TYPE L-III



STRUCTURAL SECTION AND DESIGN DETAIL
PER DPW STANDARD NO. RS-2(C)

ALL LOCAL STREETS IN SR-3 AND SR-1 ZONES
UNLESS OTHERWISE DESCRIBED.

LOCAL TYPE L-IV



STRUCTURAL SECTION AND DESIGN DETAIL
PER DPW STANDARD NO. RS-2(B)

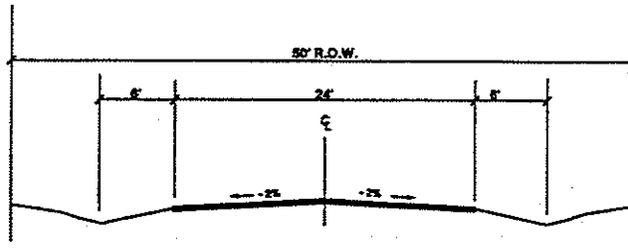
ALL CUL-DE-SAC STREETS IN R-1, R-2 AND R-3 ZONES.

North Chico Specific Plan

STREET CROSS SECTIONS

Figure 7-3d

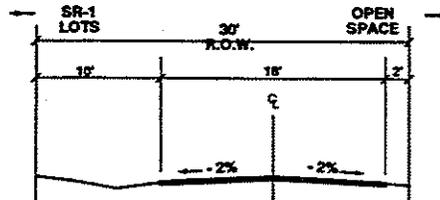
LOCAL TYPE L-V



STRUCTURAL SECTION AND DESIGN DETAIL
PER DPW STANDARD NO. RS-3(C)

CUL-DE-SAC STREETS AND LOCAL STREETS IN SR-1
AND SR-2 ZONES SERVING FEWER THAN 12 LOTS.

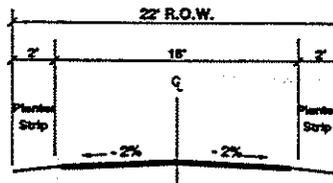
LOCAL TYPE L-VI



STRUCTURAL SECTION AND DESIGN DETAIL
PER DPW STANDARD NO. RS-3(C)

ONE-WAY LOOP STREET

LOCAL TYPE L-VII



STRUCTURAL SECTION AND DESIGN DETAIL
PER DPW STANDARD NO. RS-LD(1)

ALLEYS IN R-1 AND R-2 ZONES

North Chico Specific Plan

NORTH CHICO SPECIFIC PLAN

Street Landscaping

Street trees and shading requirements apply to all new development. Street trees must be planted within six months of the completion of streets. Tree species should be selected based on their ability to grow well within the planning area and their ability to provide maximum pavement shading. The following is a planting list for streets within the Plan area:

- Local (Rural) Street Species can be selected by the developer and property owners. Trees should have a spreading structure and be planted 30 to 50 feet apart depending on species.
- Rural Collector Street. This street will be planted with California Sycamore (*Platanus racemosa*) 40 feet on center between Garner Lane and Hicks Lane. Existing English walnuts can be used as an alternative.
- Local Urban Street. Local urban streets in the core will be planted with several varieties of trees of moderate size that do not raise or break concrete curbs and sidewalks. Individual streets should be lined with no more than two species. Acceptable varieties are: Goldenrain Tree (*Koelreuteria paniculata*), Bradford Pear (*Pyrus calleryana*), Chinese Pistache (*Pistacia chinensis*).
- Collector Urban Street. The street looping through the Village Core will be planted with Chinese Hackberry (*Celtis sinensis*).
- Arterial Streets. The New Arterial will be planted with London Plane Tree (*Platanus acerifolia*) from Eaton Road to Garner Lane; except the area between Sycamore Creek and the Industrial area will be preserved as resource conservation area and will have street trees only in the center median. Existing walnut trees will be used from Garner Lane to State Route 99. Garner Lane and Keefer Road will be planted as desired by property owners. The realignment of Hicks Lane from the new arterial street to Mud Creek will be planted with London Plane.
- The Industrial streets will be planted with London Plane trees (*Platanus acerifolia*).

Planter Strips between streets and sidewalks will be planted and maintained by the adjacent property owners. Lawn or drought resistant planting no higher than one foot can be planted by the property owner.

Alleys. The section for alleys leaves a two foot planter between the paved section and the back yard because this area should be planted with shrubs or flowers.

Pathway Design

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Paved Multi-Use Paths will be paved to a minimum 8-foot width with 2 inches of asphalt on six inches of base work or with four inches of concrete. Paths next to streets without drainage swales should be located 5 feet beyond the street tree. The path should curve back to the street at intersections to form a typical street crosswalk configuration. The landscaping between the street and path will be local annual grasses. Paths next to streets in parks or areas of special biological significance will be 12 feet from the edge of the street. The vegetation between the road and the street will be consistent with the park or area of biological significance.

Unpaved Trails provide access for those on foot, horseback, or bicycle. Trails will have a minimum width of 10 feet except for short sections where existing trees may restrict.

The surface may be earth or gravel but must be well enough drained to be passable during wet weather. Trails will generally follow the creeks and greenways, providing an attractive alternative circulation system apart from streets.

Street and Pathway Lighting

The following guidelines address nighttime illumination on streets and paths. All streets and paths will be illuminated in the Village Core, industrial areas and the areas south of Sycamore Creek. In all instances, lighting shall be the minimum intensity necessary to achieve its intended purpose. Downward oriented, cut-off type fixtures and shielding shall be used in order to prevent light spillage and glare impacts beyond the target of illumination. Lighting for pedestrian pathways and parking areas shall illuminate only the pavement. Use of low, bollard-type fixtures is encouraged. Energy conservation shall be a prime consideration when designing any lighting system. Photocell operation is mandatory to ensure efficient use of energy and minimize unnecessary usage. Open space areas shall not be illuminated either directly or indirectly by light spillage from outside light sources.

PARKS AND OPEN SPACE DESIGN STANDARDS

The design standards for parks and open space are intended to maintain the rural character and protect and enhance the natural wildlife habitat of the Plan area.

Neighborhood Park

The following are design guidelines for the neighborhood park.

- The park design should be integrated with the adjacent elementary school and school playfields.
- The area of the park located west of the school will be the site for tennis courts and basketball facilities as well as a large open space area that will serve as spillover parking for the park school and village commercial core.

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- The park could include the following facilities: a full size soccer field, a softball field, picnic area and shelter restrooms, area for children play apparatus, 3 tennis courts, a basketball court, 2,000 to 3,000 square feet indoor recreational room, and parking.
- Landscaping material should be chosen for low water use.
- The use of native shrubs and trees is encouraged.
- Trees around turf areas should be chosen to tolerate turf grass watering. The trees should have adequate size and spreading ability to form a solid backdrop to the playfield in order to create a meadow effect.
- Trees along paths, on the west side of the park and low water areas should be local natives such as Oaks and Black Walnuts and Sycamores.
- Visual access into park should be maintained from adjacent areas to reduce crime and vandalism. Fencing and/or vegetation should provide for such access.
- Drainage swales in the park should be extremely shallow with natural grass, irrigated turf or sand bottoms.
- Lighting will be limited to street and path lights. (See section on lighting)
- No provisions will be made to light playfields or courts for nighttime use in order to preserve the rural character of the area.

Village Core Greenbelt

The major Greenbelt runs north-south from the southern tip of the village core through the park north to Mud Creek. Only the New Arterial Road and the looping collector road near the southerly end interrupt this central greenbelt. The Village Core greenbelt is connected to the balance of the open space system via additional greenbelt corridors on all sides. These greenbelts all have an 8-foot wide bicycle and pedestrian path with drainage swales designed as periodic stormwater detention areas.

The landscaping will be low water requirement with Oaks and native shrubs where possible. Trees that tolerate irrigation will be used around turf grass areas. Trees should be planted to create alternating open and tree covered areas. Sixty percent of paths should be shaded by the mature landscape. The greenbelt overlaying the Hicks Lane alignment will have a dense planting of evergreen to serve as a visual buffer between the village core and the airport. Redwoods, Stone Pines and Kashurina will be planted with a minimum of two rows and less than 10-to 15-foot on center depending on species.

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Drainage swales should have natural grass, irrigated grass or sand bottoms. Grading should allow for the direct runoff from adjacent properties and streets. Swales should be shallow and wide without steep banks. In mini-parks the swales will have sand or irrigated turf grass bottoms.

Lighting will be limited to street or path lights. (See section on lighting) No additional lighting will be used.

Corridors

Street-side Path and Drainage Corridors: These corridors will be 20 feet minimum width, along road rights-of-way where indicated in Figure 4-2, and will complete important path and drainage linkages. They provide for an 8-foot wide bicycle and pedestrian path and additional room for drainage and tree plantings for buffers.

Landscaping will be predominantly the chosen street tree corresponding to the adjacent street and natural grass. The grass will need to be field-mowed two or three times a year for aesthetics and to reduce fire hazard.

A buffer of densely planted evergreen trees will be planted in the corridor running north of the realignment of Hicks Lane from the new arterial west to the industrial greenbelt on the old alignment of Hicks Lane. Two rows of trees planted 10 to 15 feet on center will be planted using Redwoods, Stone Pines, and Kashurina.

Drainage swales in these corridors will be away from the street on the outside of the bike path. They will be shallow with a natural grass or gravel bottom. If adjacent to residential lots, property owners may plant irrigated lawn in the bottom of the wall and to the edge of the paved path.

Trail and Drainage Corridors: There are two of these corridors, one connecting Mud Creek and Keefer Slough and one going north from Keefer Slough on the west side of Garner Lane. These corridors are to be planted with native shrubs and trees and natural grasses. They will have shallow swales and may have stormwater detention grass basins. They will have no lighting. They should ultimately look like natural areas with trail and paved paths.

Creek Corridors

All natural water corridors will have a 10 foot trail dirt or gravel but must be passable in wet weather. Should be located to disrupt wildlife and vegetation as little as possible.

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Wetlands

Where streets cross wetlands there should be no street trees (except in center median, where applicable), natural grass should extend to edge of paved section or bicycle and pedestrian path and the area between the path and street should be natural grasses, street and paths should sheet drain into the wetlands with proper engineering design and pollution control devices.

VILLAGE DESIGN GUIDELINES

The following section provides design guidelines and standards intended to preserve the rural character of the Plan area. The NCSP character is accentuated by the establishment of a Village with design standards that reflect traditional small valley towns. A critical element in the preservation of the rural character of the area will be keeping automobile traffic to a minimum. A combination of the centrally located Village Core easily accessible by walking and bicycling along with design standards that create safe and aesthetically pleasing routes should help to reduce automobile dependency.

Residential Design

The North Chico Specific Plan provides for a variety of housing types ranging from single-family low-density to multi-family. Streets have been laid out so most lots have a north-south orientation for possible solar design. Street trees were selected and streets laid out to assure solar access in winter and shading in summer.

The major design influence will come from the street and sidewalk layout, and also from the setbacks and orientation of structures and their front doors and garages to streets, paths, and alleys.

Multi-Family Design

The design standards for multi-family are intended to create structures that appear more like townhouses.

- All multi-family units must face with front doors onto streets, greenbelts or park area.
- Parking lots are to be on the interior of building clusters and not on the street side of the structure.
- Walkways to front doors will be from street side walks or from greenbelt or park paths.
- There should be no fencing over 3 feet high between apartments and street sidewalks or greenbelt or park paths.
- Parking in multi-family will have shade trees no less than 40 feet on center.
- Trash containers must be enclosed and located within the center of building complexes.

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Village Core Commercial Design Guidelines

The Village Core commercial area has been designed to reduce dependence on the automobile, and encourage pedestrian and bicycle travel. The layout provides for a complete and integrated community continuing housing, shops, work places, schools, parks and civic facilities essential to the daily life of the residents. The site is designed so that housing, jobs, shopping, recreation and other activities are within easy walking distances of each other.

The design standards below are intended to produce an aesthetic composition of buildings and open space that will encourage frequent use and to create a more traditional village form and prohibit the standard strip shopping center look.

Building Siting

- Buildings shall face on street sidewalks and plazas. Parking shall be either on street or in back or side parking lots — not in parking lots directly adjacent to streets. Buildings shall be sited to emphasize their relationship to street plazas and the village green (part of the neighborhood park).

Office Use

Office Use occurs on one site in the core.

- Buildings on this site shall face directly on to the street with parking on the side or in back.
- A 20-foot landscaped setback is required between the building and sidewalk.
- Entrances to the building must provide access to all sidewalks and the greenbelt.

Architectural Details

- Street level stores should use awnings or have sidewalk cover.
- Second level office or residential units should have windows and, if possible, balconies overlooking streets and plaza.

Commercial Core Landscaping

- Street trees shall be flowering crab apple (*Florabunda*), plaza trees will be Bradford Pear.

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- Planters and screening for parking areas shall be provided.
- Plant varieties should be selected for drought tolerance.
- Parking lot shade trees will be Chinese Hackberry planted approximately 40 feet on center.

Bicycle Parking

- Bicycle racks must be provided throughout the non-residential portion of the Village Core area.

8. IMPLEMENTATION

Capital Improvement Program

Development within the North Chico Specific Plan (NCSP) area requires the completion of capital improvements as set forth in this section. The specific infrastructure requirements are described in the appropriate section of the Public Facilities and Services, Circulation, and Parks and Open Space Elements of this Plan. A summary of capital improvements and cost estimates is shown in Table 8-1.

Onsite public improvements normally required to be constructed and installed by individual developers as part of specific development proposals are not covered by this section.

Financing

The identified public improvements which are required to facilitate development of the NCSP area and to mitigate the impacts associated with development are estimated to cost approximately \$50 million dollars. The financing plan set forth herein has been developed to organize and ensure the overall funding of facilities and to provide a fair and equitable means of sharing the costs between property owners.

In devising the financing plan, all available public finance options were examined, including:

- Mello-Roos Community Facilities District,
- Special Assessment District,
- Infrastructure Financing District,
- Integrated Financing District,
- Certificates of Participation;
- County Service Area,
- Impact Fees,
- Landscaping and Lighting District.

A full description of these financing options is set forth in Appendix B.

Financing Plan Policies

Financing Plan Policies have been established to ensure that the development in the NCSP area: (a) pays the full costs of the infrastructure needed to serve the area; (b) funds the costs of mitigating adverse impacts on existing Butte County and City of Chico infrastructure, where affected; (c) provides for reimbursements from other development areas for costs that the NCSP was required to advance (if any); and (d) provides a fair allocation of costs between land uses.

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The financing policies are set forth as follows:

Policy 1: Fund the full costs of on and off-site public infrastructure and public services required to support development in the Plan area from revenues generated by development within the Plan area.

Policy 2: Use pay-as-you-go financing to the extent possible. Use debt financing only when essential to provide facilities necessary to permit development or to maintain service standards.

Policy 3: Allocate the backbone infrastructure costs to property within the Plan area based on the general principles of benefit received, with consideration to the financial feasibility of the proposed land use.

Policy 4: Require developers who proceed ahead of the infrastructure financing sequence to pay the costs of extending the backbone infrastructure to their project subject to future reimbursement.

Policy 5: Landowners who do not wish to develop will not be required to participate in debt financing that would result in a financial obligation on their property.

Benefit Spread Analysis of Capital Facilities

A benefit spread analysis has been prepared to distribute the capital facility costs on a fair share basis. This cost allocation was the basis for establishing the proposed North Chico Specific Plan Impact Fee. A summary of the spread of costs for drainage, transportation, sewer, and trails is presented on the following pages. The fees for schools and parks have been established by the Chico Unified School District and Chico Area Recreation and Park District, respectively. A fee summary is presented in Table 8-7, and a facility cost and funding summary is provided in Table 8-8.

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**TABLE 8-1
SUMMARY OF CAPITAL IMPROVEMENTS AND COSTS**

Facility Description	Total Estimated Cost	Source
Transportation Facilities:		
Hicks Lane Realignment and Widening		
<input type="checkbox"/> Acquisition of Right of Way	\$2,200,000	
<input type="checkbox"/> Construction	\$207,000	
<input type="checkbox"/> Hicks Road Bridge Replacement at Mud Creek	\$1,129,000	
New Arterial Road		
<input type="checkbox"/> Hicks Lane to Garner	\$2,705,000	
<input type="checkbox"/> Garner to SR 99	\$524,000	
Intersections		
<input type="checkbox"/> SR 99/Keefer	\$45,000	
<input type="checkbox"/> Eaton/SR 99 (SB ramps)	\$126,000	
<input type="checkbox"/> Eaton/SR 99 (NB ramps)	\$215,000	
Subtotal of Transportation Improvements	\$7,151,000	Table 8-4
Butte County: Chico Urban Area Street Facilities Fee	\$3,898,000	Table 8-2 Sched. 1
Total of Transportation Facilities:	\$ 11,049,000	
Sanitary Sewer		
<u>Alternative 1: City of Chico</u>		
<input type="checkbox"/> On-site Main Lines	\$1,611,000	
<input type="checkbox"/> Interim Outfall Lines	\$1,047,000	
<input type="checkbox"/> Truck Line to Plant	\$3,207,000	Table 8-2 Sched. 2
<input type="checkbox"/> City Treatment Fees	\$2,704,000	Table 8-2 Sched. 3
Subtotal of Alternative 1:	\$8,569,000	
<u>Alternative 2: County</u>		
<input type="checkbox"/> On-site Trunk Lines	\$1,611,000	
<input type="checkbox"/> Treatment Plant, Land, Outfall	\$6,628,000	
Subtotal of Alternative 2:	\$8,239,000	
Other Infrastructure/Systems		
Parks : Figure derived by existing CARD fee structure	\$ 5,888,000	Table 8-2 Sched. 6
Trail System: Cost estimate; allocated per dwelling unit	\$563,000	Table 8-2 Sched. 4
Schools: Figure derived by existing CUSD fee structure	\$17,368,000	Table 8-2 Sched. 7
Fire Station: Cost estimate; allocated among land uses	\$1,500,000	Table 8-6
Sheriff: Figure derived by existing County fee structure	\$1,057,000	Table 8-2 Sched. 8
Storm Drainage: Cost estimate; allocated among land uses	\$1,435,000	Table 8-5
Total of other Infrastructure/Systems	\$27,811,000	
Total with Sanitary Sewer Alternative 1:		\$47,516,000
Total with Sanitary Sewer Alternative 2:		\$47,186,000

Table 8-2, *Improvement Cost Detail and Allocation Among Land Uses*, is presented in 8 separate schedules showing the spread of costs for different infrastructure improvements and fees.

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Table 8-2 - Schedule 1. Chico Urban Area (CUA) Street Facilities Fee			
Land Use	DU or SF	Fee per Unit	Total amount
SR-3	35 DU	\$1,331	\$46,585
SR-1	719 DU	\$1,331	\$956,989
R-1	912 DU	\$1,331	\$1,213,872
R-2	829 DU	\$804	\$666,516
R-3	308 DU	\$804	\$247,632
Industrial	2,153,000 SF	\$0.10	\$215,300
Commercial	268,000 SF	\$1.75	\$469,000
Office	283,000 SF	\$0.29	\$82,070
Total CUA Street Facilities Fee			\$3,897,964

Table 8-2 - Schedule 2. Chico Urban Area Sewer Trunk Line Capacity Fees			
Land Use	DU or Acres	Fee per Unit	Total Amount
R-1	912 DU	\$977	\$891,024
R-2	829 DU	\$977	\$809,933
R-3	308 DU	\$619	\$190,652
Industrial	247.1 AC	\$4,418	\$1,091,781
Commercial	24.6 AC	\$4,418	\$108,683
Office	26.0 AC	\$4,418	\$114,868
Total			\$3,206,941

Table 8-2 - Schedule 3. Chico Urban Area Sewer Treatment Plant Fees			
Land Use	DU or Acres	Fee per Unit	Total Amount
R-1	912 DU	\$824	\$751,488
R-2	829 DU	\$824	\$683,096
R-3	308 DU	\$522	\$160,776
Industrial	247.1 AC	\$3,735	\$920,526
Commercial	24.6 AC	\$3,735	\$91,635
Office	26.0 AC	\$3,735	\$96,850
Total			\$2,704,371

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Table 8-2 - Schedule 4. Trail System			
	LF-Unit	\$/LF-Unit	Total Amount
Path of Trail (LF*\$ /LF)	34,000	\$6.00	\$204,000
Off Street Path (LF*\$ /LF)	18,000	\$8.50	\$153,000
Three Foot/Bike Bridges	3.00	\$20,000	\$60,000
<i>Subtotal Trail System Direct Costs</i>			\$417,000
Construction contingency	20%	\$3,735	\$83,400
Engineering and Administration	15%	\$3,735	\$62,550
Total Trail System			\$562,950

Table 8-2 - Schedule 5. Neighborhood Parks			
	Acres	\$/Acre	Total Amount
Land Acquisition	15.00	\$25,000	\$375,000
Capital Improvements	15.00	\$75,000	\$1,125,000
Community Building (5,000 sf)			\$600,000
Total Neighborhood Park Facilities Cost			\$2,100,000

Table 8-2 - Schedule 6. Chico Area Recreation District (CARD) Park Fees			
Land Use	DU or SF	Fee per Unit	Total amount
SR-3	35 DU	\$2,126	\$74,410
SR-1	719 DU	\$2,126	\$1,528,594
R-1	912 DU	\$2,126	\$1,938,912
R-2	829 DU	\$2,126	\$1,762,454
R-3	308 DU	\$1,799	\$554,092
Industrial	2,153,000 SF	\$0.01	\$21,530
Commercial	268,000 SF	\$0.02	\$5,360
Office	283,000	\$0.01	\$2,830
Total CUA Street Facilities Fee			\$5,888,182

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Land Use	Avg. DU size	Fee/SF	Fee per Unit	# DU	Total amount
SR-3	2,400	\$1.84	\$4,416	35	\$154,560
SR-1	2,100	\$1.84	\$3,864	719	\$2,778,216
R-1	1,750	\$4.60	\$8,050	912	\$7,341,600
R-2	1,600	\$4.60	\$7,360	829	\$6,101,440
R-3	700	\$4.60	\$3,220	308	\$991,760
Total					\$17,367,576

Note: Fee per unit will vary based upon square footage. See list of assumptions at the end of this chapter.

Land Use	DU or SF	Fee per Unit	Total amount
SR-3	35 DU	\$360	\$12,600
SR-1	719 DU	\$360	\$258,840
R-1	912 DU	\$360	\$328,320
R-2	829 DU	\$360	\$298,440
R-3	308 DU	\$252	\$77,616
Industrial	2,153,000 SF	\$0.03	\$64,590
Commercial	268,000 SF	\$0.03	\$8,040
Office	283,000	\$0.03	\$8,490
Total			\$1,056,936

NORTH CHICO SPECIFIC PLAN

**TABLE 8-3
SEWER ALTERNATIVE 2 FEES**

Land Use	Acreage	ADDWF Acre Per Day	Total Avg Dry Weather Flow (Gal.)	% of Total	Spread of Cost by Use	Approx. Cost Per DU or SF
R-1	288.6	1,150	268,985	28.9%	975,532	\$879 Unit
R-2	118.4	1,300	153,920	16.5%	244,656	\$616 Unit
R-3	17.1	2,900	49,590	5.3%	342,704	\$534 Unit
Commercial	24.6	1,500	36,900	4.0%	184,343	\$0.46 sf
Industrial	247.1	1,500	370,682	39.8%	1,179,052	\$0.57 sf
Office	26.0	1,500	39,000	4.2%	101,450	\$0.46sf
School	8.5	1,500	12,750	1.4%	N/A	N/A
Total			931,827	100.0%	\$3,093,000	

Spread of Transportation Costs

Improvements:

Hicks Lane Realignment and Widening	\$3,536,000
New Arterial Road	\$3,229,000
Intersection	<u>\$386,000</u>
Total:	\$7,151,000

Potential industrial land within City limits adjacent to airport served by New Collector Road and bridges: 162 Ac. Industrial land adjacent to the airport and within the Chico City limits will not be able to develop until the transportation improvements for the area have been completed. These City lands will generate the same number of trips per acre as lands within the County and should be required to pay their fair share towards these improvements. See Table 8-4

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TABLE 8-4 SPREAD OF TRANSPORTATION COSTS

Land Use	Number of Units	Daily Trips per Unit	Total Trips	% of Total	Spread of Costs by Land Use	Number of DU or SF	Approx. Cost Per DU or SF
SR-3	35 DU	9.55/DU	334	0.5%	\$36,223	35 DU	\$1,035/DU
SR-1	719 DU	9.55/DU	6,866	10.4%	\$744,118	719 DU	\$1,035/DU
R-1	912 DU	9.55/DU	8,710	13.2%	\$943,860	912 DU	\$1,035/DU
R-2	829 DU	9.55/DU	7,917	12.0%	\$857,960	829 DU	\$1,035/DU
R-3	308 DU	6.47/DU	1,993	3.0%	\$215,956	308 DU	\$701/DU
Commercial	268 KSF	54.5/KSF	14,600	22.1%	\$1,582,227	268,000 SF	5.90/SF
Industrial	247 Acres	51.8/AC	12,801	19.4%	\$1,387,231	2,153,000 SF	0.64/SF
Industrial*	162 Acres	51.8/Ac.	8,392	12.7%	\$909,398	1,411,344SF	0.64/SF
Office	283 KSF	14.03/KSF	3,972	6.0%	\$430,495	283,000 SF	1.52/SF
School	30 Emp.	13.39	402	0.6%	\$43,532	N/A	
Total			65,987	100.0%	\$7,151,000		

* Note: City of Chico will be required to adopt a \$0.64/SF fee for development within City industrial lands west of the airport.

Spread of Drainage Costs

TABLE 8-5

Land Use	Acreage	SCS Curve Number	Acreage Curve Number	% of Total	Spread of Costs	Number of DU or SF	Approx. Cost DU/SF Per Unit or SF
SR-3*	109.8			0.8%	12,000	35	\$343 DU
SR-1*	824.6			20.1%	288,000	719	\$401 DU
R-1	233.9	83	19,414	26%	419,267	912	363 DU
R-2	118.4	85	10,064	13.4%	5,216	829	213 DU
R-3	17.1	90	1,539	2.1%	63,326	308	88 DU
Commercial	24.6	94	2,313	3.2%	68,769	268,000	0.15 sff
Industrial	247.1	91	22,488	30.1%	425,901	2,153,000	0.15 sf
Office	26.0	94	2,444	3.2%	37,842	283,000	0.15 sf
School	8.5	94	799	1.1%	24,679	N/A	
Total			59,060	100.0%	\$1,435,000		1,645 Acre

* Note: \$300,000 for SR-1 and SR-3 areas to be used for construction of facilities as identified in the Hazard Mitigation Grant program application for Keefer Slough drainage improvements.

8-8 Implementation

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Spread of Trail and Pathway Costs

The cost of trails and pathways were spread over all residential property on a per unit basis. The total residential unit count including all residential categories is 2,803 units. The total estimated cost of trail and pathway facilities is \$562,950. The total cost share per residential unit is $\$562,950/2,803 = \$201/\text{unit}$. (See Table 8-2 Schedule 4.)

Spread of Fire Station Costs

Fire protection costs are spread over the NCSP area based on the number of residents or employees. Employees are determined to benefit by 1/3 of that of people living in residential units.

**TABLE 8-6
SPREAD OF FIRE STATION COSTS**

Land Use	Units DU or SF	Res./DU or SF/Employ	Residents or Employees	% Population by Land Use	Cost by Land Use	Cost per DU or SF
SR-3	35 DU	3.0	105	1.1%	\$17,094	\$493 DU
SR-1	719 DU	3.0	2,157	23.4%	\$351,169	\$493 DU
R-1	912 DU	3.0	2,736	29.7%	\$445,433	\$493 DU
R-2	829 DU	2.5	2,073	22.5%	\$337,412	\$411 DU
R-3	308 DU	2.25	693	7.5%	\$112,824	\$370 DU
Industrial	2,153,00	750	957	10.4%	\$155,786	\$0.07 SF
Office	283,000	300	314	3.4%	\$51,193	\$0.18 SF
Commercial	268,000	500	179	1.9%	\$29,088	\$0.11 SF
Total			9,214	100.0%	\$1,500,000	

Note: Non-residential uses assume a 1/3 of employee (8 hour work day) to calculate population, e.g. 2,153,000 sf divided by 750 sf/employees divided by 1/3 (work day) equals 957

Finance Strategy Plan

Area North of Mud Creek

The phasing of capital facilities is not regulated in this sector of the NCSP area. Individual project proponents will fund and construct roads, water, and drainage facilities to the extent necessary to meet the impacts of their specific projects.

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TABLE 8-7

Land Use	Acres	DU or SF	Chico Street Facility	Chico Sewer Trunkline	Chico Treatment Plant	Trails System	Parks (CARD)	School (CUSD)	Sheriff (County)	Roads & Bridges	Storm Drainage	Fire Station	Total w/ Sewer Alt. 1
Table Reference			8-2 (1)	8-2 (2)	8-2 (3)	8-2 (4)	8-2 (6)	8-2 (7)	8-2 (8)	8-4	8-5	8-6	
Residential													
SR-3	109.8	35	\$1,529	n/a	n/a	\$201	\$2,126	\$4,416	\$360	\$1,035	\$343	\$493	\$10,305/DU
SR-1	824.6	719	\$1,529	n/a	n/a	\$201	\$2,126	\$3,861	\$360	\$1,035	\$401	\$493	\$9,810/DU
R-1	233.9	912	\$1,529	\$977	\$824	\$201	\$2,126	\$8,050	\$360	\$1,035	\$409	\$493	\$15,857/DU
R-2	118.4	829	\$1,529	\$977	\$824	\$201	\$2,126	\$7,360	\$360	\$1,035	\$233	\$411	\$14,349/DU
R-3	17.1	308	\$956	\$619	\$522	\$201	\$1,799	\$3,220	\$252	\$701	\$96	\$370	\$8,591/DU
Total Residential Units	1,303.8	2,803											
Industrial/Commercial													
Industrial	247.1	2,153,000	\$0.56/SF	\$0.51/SF	\$0.43/SF	n/a	\$0.01/SF	n/a	\$0.03/SF	\$0.64/SF	\$0.20/SF	\$0.07/SF	\$2.01/SF
Commercial	24.6	268,000	\$5.13/SF	\$0.41/SF	\$0.34/SF	n/a	\$0.02/SF	n/a	\$0.03/SF	\$5.90/SF	\$0.17/SF	\$0.11/SF	\$8.74/SF
Office	26.0	283,000	\$1.12/SF	\$0.41/SF	\$0.34/SF	n/a	\$0.01/SF	n/a	\$0.03/SF	\$1.52/SF	\$0.17/SF	\$0.18/SF	\$3.96/SF
Total Industrial/Commercial	271.1	2,421,00											

**TABLE 8-8
NORTH CHICO SPECIFIC PLAN FACILITY COST AND FUNDING SUMMARY**

Facility Type	Total Estimated Cost	County, CARD Fees	NCSP Impact Fees	School Fees	City Sewer Fees	Mello-Roos or Assess.	City Contribution
Transportation Facilities							
Hicks Realignment and widening	\$3,536,000		\$3,086,000				\$450,000
New Arterial Road Intersections	\$3,229,000		\$2,818,000				\$410,000
Chico Urban Area Street Facility Fees	\$386,000		\$337,000				\$49,000
	\$3,898,000	\$3,898,000					
Sub-total Transportation Facilities	\$11,049,000	\$3,898,000	\$6,241,000				\$910,000
Sanitary Sewer							
Alternative 1: City of Chico							
Onsite Main Lines	\$1,611,000					\$1,611,000	
Trunk Line to Lift Station	\$1,047,000				\$3,207,000	\$1,047,000	
City Trunk Line Fees	\$3,207,000				\$2,704,000		
Total Sewer Alternative #1	\$8,569,000				\$5,911,000	\$2,658,000	
Alternative 2: County							
On-site Turnk Lines	\$1,611,000		\$815,000			\$796,000	
Package Plant, Mains, Outfall	\$6,628,000		\$2,278,000			\$4,350,000	
Total Sewer Alternative #2	\$8,239,000		\$3,093,000			\$5,146,000	
Storm Drainage	\$1,435,000		\$1,435,000				
Parks (CARD Fee)	\$5,888,000	\$5,888,000					
Trail System	\$563,000		\$563,000				
Schools	\$17,368,000			\$17,368,000			
Fire Station	\$1,500,000		\$1,500,000				
Sheriff	\$1,057,000	\$1,057,000					
Total with Sanitary Sewer Alt. 1	\$47,616,000	\$10,843,000	\$9,926,000	\$17,368,000	\$5,911,000	\$2,658,000	\$910,000
Total with Sanitary Sewer Alt. 2	\$47,186,000	\$10,843,000	\$12,919,000	\$17,368,000		\$5,146,000	\$910,000

Any of these facilities that are a part of the fee funded or finance district funded facilities will be eligible for credits and/or reimbursements. Eligible facilities will include road improvements and drainage easement/greenbelt linear facilities (see Figure 8-1).

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If sewers are required and/or desired along State Route 99 north of Sycamore Creek, it is recommended that a Special Assessment District be formed in this region over all benefiting property (as shown in Figure 8-2) to fund the sewer facilities described in the Public Facilities and Services Element and shown in Figure 6-2.

Area West of Garner Lane

Development of the ±350 acres south of Keefer Slough and west of Garner Lane is now in agricultural uses among four ownerships. Although limited development may occur in multiple phases among the properties, it may be desirable to facilitate construction of the new collector road from Garner to SR 99. It is recommended that a Special Assessment District be formed to fund an expedited construction of this road segment. As with similar facilities in other areas, appropriate improvements will be eligible for credits and/or reimbursements from the NCSP Impact Fees.

Area from Eaton Road to Mud Creek (including the Village Core)

The development in this region will require the formation of a Mello-Roos Community Facilities District to fund sewer outfall and/or treatment facilities. It is proposed that funding will occur in two bond sales as development occurs.

Bond Sale No. 1 (Prior to Development):

This initial bond sale will fund the initial sewer facilities to provide service to this area (see Figure 8-3). The amount of this funding will depend upon the alternative selected for sewer service.

Bond Sale No. 2 (At approximately 800 residential unit buildout):

The second bond sale will fund the construction of the southerly leg of the New Collector Road, Sycamore Creek Bridge, and the link from this collector to Hicks Lane (see Figure 8-4).

All other capital facilities will be funded by a fee program.

Operations and Maintenance Program

The operations and maintenance of "expand-ed services" within the NCSP area will be provided by a County Service Area (CSA).

Either the existing County Service Area No. 87 may be amended to include the proposed authorized services and the entirety of the NCSP area, or a new CSA may be established. Procedures for formation of a CSA are described in Appendix B.

NORTH CHICO SPECIFIC PLAN

The intended services to be provided by the CSA are as follows:

Parks and Greenbelts: The CSA shall provide funds to CARD or shall contract for services with a private maintenance contractor to provide park and greenbelt maintenance. Budgets for annual maintenance shall be determined each year based upon anticipated costs and the level of park and greenbelt area that has been developed at that time. It is recommended that the first year budget be set at \$10,000. The annual assessment for parks and greenbelt services shall be allocated within the NCSP area on the basis of acreage.

Storm Drainage Maintenance: The CSA shall provide funds to Butte County to provide expanded storm drainage maintenance services. The expanded services include the linear drainage swales and greenbelt. Normal roadside ditch maintenance is not considered expanded services, however, should local residents desire a higher level of service than provided by the County, funds may be budgeted for this purpose. It is recommended that the first year budget be set at a minimum of \$10,000. The cost for storm drainage maintenance services shall be allocated on an acreage basis within the NCSP area.

Community Building Maintenance: The NCSP identifies a 5,000-square-foot community building to be developed within the neighborhood park south of Mud Creek. The CSA will provide the funds for annual maintenance of this facility. The facility will be owned by the County and constructed with CARD fees allocated to the NCSP area. No initial budget has been identified for this facility. At the time of development of the facility, an annual budget can be established. The cost for community building maintenance shall be allocated on an acreage basis.

Interim Fire Station: The CSA will provide funds to the Butte County Fire Department to supplement staff at existing fire facilities until the new fire station is constructed within the NCSP area. The proposed first years budget for the services is \$40,000. The cost for interim fire protection services shall be allocated on an acreage basis. At 75% buildout of the Plan area, including industrial, commercial, office, and residential, the permanent \$1.5 million station shall be complete.

Alley Maintenance: The CSA shall provide funds to the County or to a private maintenance contractor to maintain alleys. The CSA shall establish a zone of benefit for alley maintenance to include only those benefiting properties that receive access from the alleys. The CSA zone and the annual budget for alley maintenance shall be developed at the time of final map when any alleys are established. The first year's projected budget for the alleys established shall be deposited with the County at the time of recording of the final map.

The level of service for the maintenance shall be based upon the present level of service provided for the County roads in the area, however, if local residents desire a higher level of service, the CSA budget can be adjusted accordingly.

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Project Phasing

Area Northwest of Mud Creek

This region of the NCSP is made up of numerous property owners. Development will occur within the region as driven by market forces and the individual desires of property owners.

The only capital facilities to be developed within this region are the New Collector Road, the realignment of Hicks Lane (north of Mud Creek), storm drainage greenbelts, and mini-parks. These facilities will be constructed by individual project proponents as needed to meet the impacts of their specific projects. The financing plan identifies a fee program for each of these elements to provide an equal basis for sharing costs to all the properties in this region. No specific phasing of infra-structure is proposed for this region. Phasing will be driven by the locations of projects and their specific need for facilities. Parks and trails will be developed over time as the fee pool of funds allows.

Projects that fall within the FEMA 100-year flood plain will be required to mitigate the flooding area on their project through project design. No areawide flood control or drainage facilities (other than drainage greenbelts and easements) are provided for in the capital facilities plan.

Area Eaton Road to Mud Creek

This area has a limited ownership and is better suited for a regional approach to capital facilities phasing. For any development to occur in this region, sanitary sewer service must be available. Additionally, the intersection of Hicks Lane and Eaton Road would need to be improved. With the improvement of this intersection, up to 800 new residential units can be accommodated on Hicks Lane prior to the need to construct a leg of the New Collector Road across Sycamore Creek. The following phasing plan describes only those facilities that need to be debt financed through a districtwide financing district. Facilities that will be funded through a fee program will phase according to need. In the event a project needs a facility ahead of the fee accumulation, the project proponent will be required to privately finance the facility with a reimbursement agreement to be repaid from subsequent fees. The developer will be able to take credits offsetting any eligible facilities the developer funds.

Phase 1

Sanitary Sewer

Construct initial interim facilities to the City of Chico system or construct initial package wastewater treatment plant. Initial capacities should accommodate 0.50 million gallons per day. The financing plan proposes that the initial sewer facilities cost be funded through the formation of a Mello-Roos Community Facilities District or Special Assessment District over all the benefiting property in this Eaton Road/Mud Creek region. The initial sewer system would include

NORTH CHICO SPECIFIC PLAN

on-site trunklines extended north across Sycamore Creek to serve the urban area between Sycamore Creek and Mud Creek (see Figure 8-3).

All subsequent sanitary sewer facilities (including treatment) will be phased as need demands and will be funded through a sewer fee program. Facilities that need to be installed in advance of sufficient fee funds will be the responsibility of the project requiring service. However, developers who advance funds for eligible facilities will be reimbursed on a first come basis as funds are available.

The timing for the extension of a 10" diameter trunk main from Eaton Road northerly along SR 99 to serve the existing commercial and industrial properties north of the SR 99/Esplanade intersection shall be determined based upon the need for sewer service. All property within this trunk extension sewer limits (see Figure 8-2) will be required to participate in an Special Assessment District to fund the facilities.

The Assessment District will include properties both inside and outside the NCSP area. The facilities to be funded with this Assessment District are 10" diameter trunk main, pump station, force main, and a pro-rata contribution to the regional outfall and treatment costs.

Transportation

As mentioned above, the only improvement required to enable development to start where there are resultant traffic impacts to Hicks Lane is the improvement of the intersection of Hicks Lane and Eaton Road. The improvement of this intersection will allow up to 800 new residential units to be developed with Hicks Lane as the only northerly access across Sycamore Creek. It is proposed that the intersection of Hicks Lane and Eaton Road be included in the initial financing district.

Phase 2

Transportation

At an approximate 800-unit buildout bench mark, the financing district will fund the first leg of the New Collector Road, the bridge across Sycamore Creek, and an extension of the link road from the New Collector Road to Hicks Lane (see Figure 8-4).

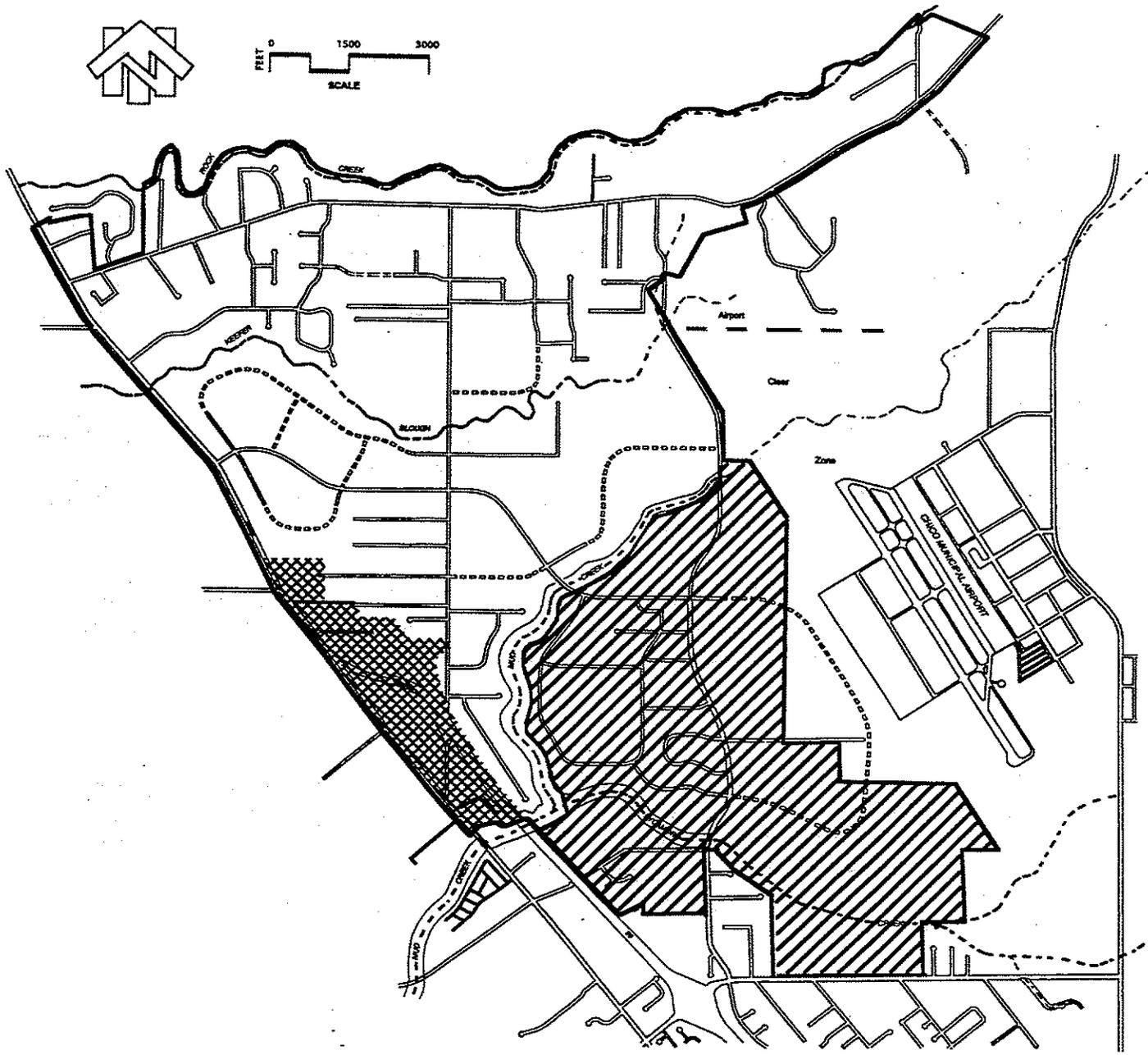
Required Development Regulations

The following regulations or ordinances must be adopted in order to ensure the successful implementation of the NCSP:

- Development Standards



0 1500 3000
FEET
SCALE



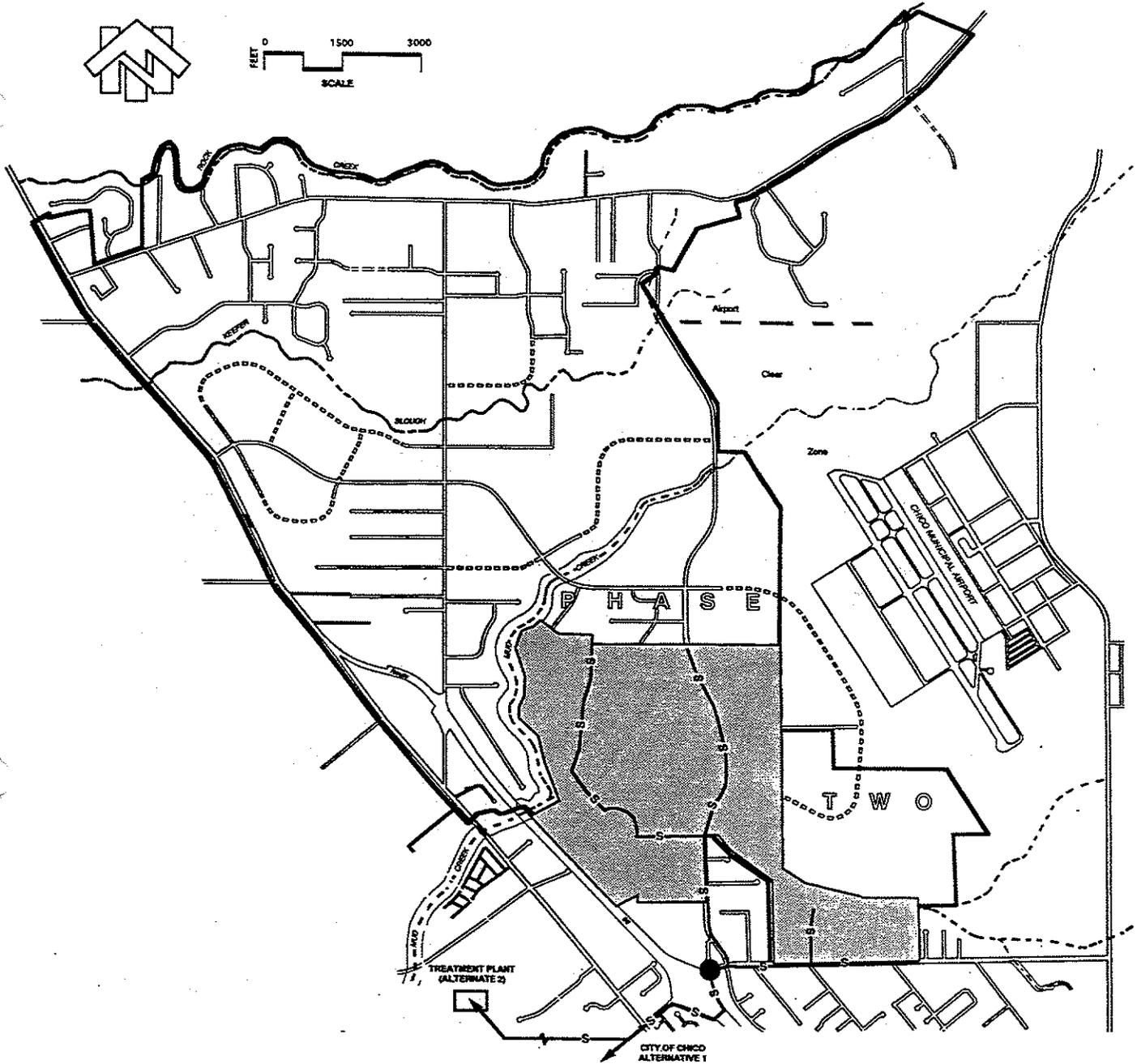
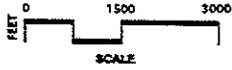
 COMMUNITY FACILITIES DISTRICT
(APPROXIMATE BOUNDARIES)

 SPECIAL ASSESSMENT DISTRICT
(APPROXIMATE BOUNDARIES)

FINANCING DISTRICTS

Figure 8-2

North Chico Specific Plan



 APPROXIMATE LIMITS OF PHASE 1 DEVELOPMENT
 (Note: Phases are for infrastructure planning and financing purposes only, and are not intended to be limiting or binding)

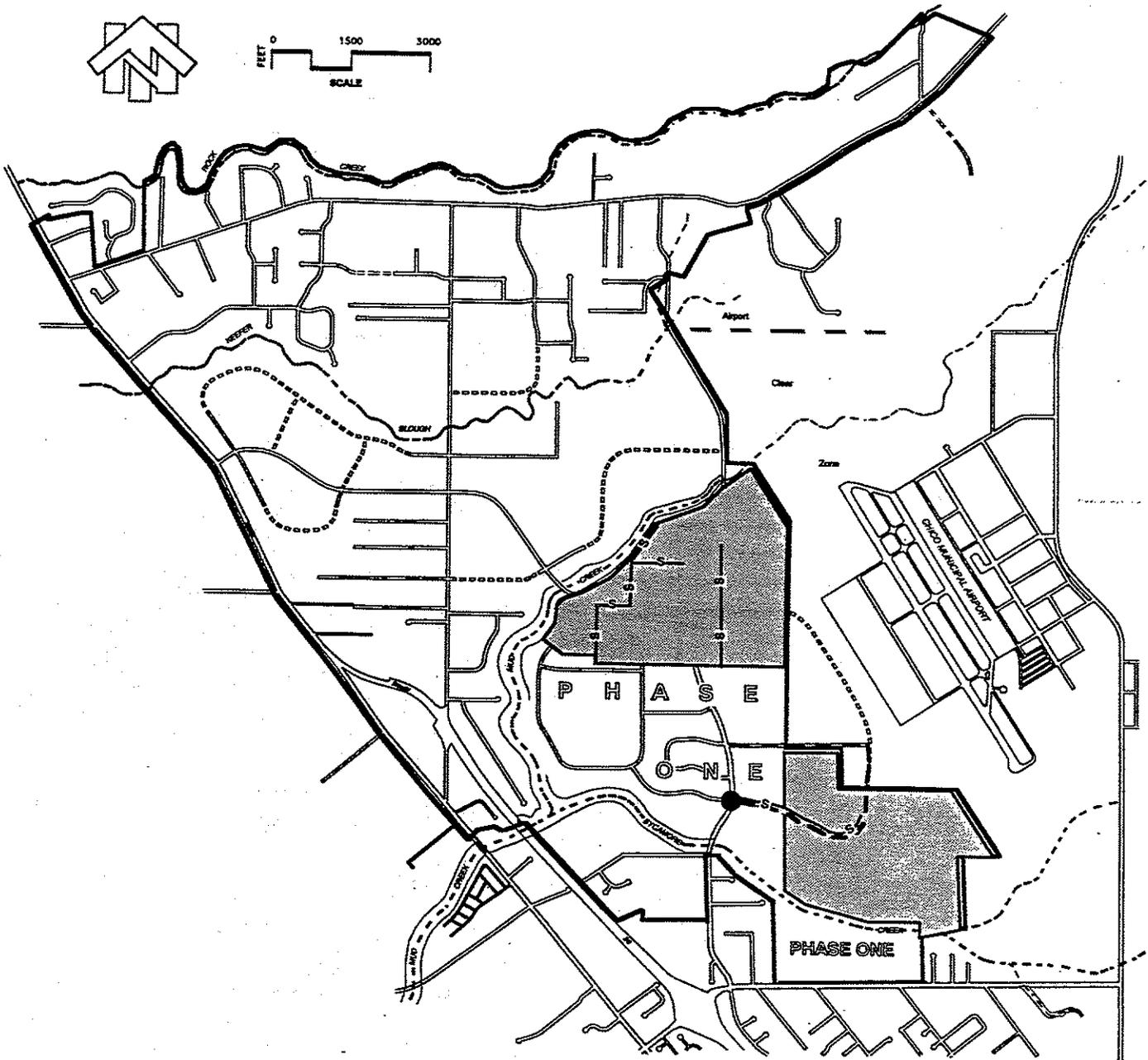
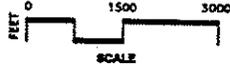
—S— PHASE 1 SANITARY SEWER

 INTERSECTION IMPROVEMENTS

MELLO-ROOS PHASE 1 IMPROVEMENTS

Figure 8-3

North Chico Specific Plan



APPROXIMATE LIMITS OF PHASE 2 DEVELOPMENT

(Note: Phases are for infrastructure planning and financing purposes only, and are not intended to be limiting or binding)

— S — PHASE 2 SANITARY SEWER

● INTERSECTION IMPROVEMENTS

--- FIRST LEG OF NEW COLLECTOR ROAD (AND LOOP TO HICKS LANE)

MELLO-ROOS PHASE 2 IMPROVEMENTS

Figure 8-4

North Chico Specific Plan

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- Design Standards (including architectural town center theme, fences and walls, lighting, signs, lighting, loading, trash and storage areas, screening, and outdoor storage)
- Design Review
- Landscape Standards
- Site Plan Review

Subsequent Development Entitlements

The NCSP will be implemented by processing projects to ensure conformity with the Plan regulations and responsiveness to applicable guidelines. The basic procedures specified for processing of projects are:

- 1) Site plan review for development within overlay and underlying zone districts within the NCSP area.
- 2) Tentative parcel or subdivision maps for the processing of any project requiring the creation of separate lots.
- 3) Use permit approval for projects that include uses identified within the various designations as conditionally allowed.
- 4) Design review and approval for commercial and industrial projects, and projects within the DR-OZ-3 (Design Review Overlay Zone) district.

Site Plan Review Procedures

The procedures for review of site plans shall be prepared at the direction of the Director of Development Services following approval of the NCSP.

Tentative Parcel Map Procedures

Chapter 20 of the Butte County Code relating to SUBDIVISIONS and IMPROVEMENT STANDARDS shall apply to all development within the NCSP area that is required to obtain approval of subdivision maps for the finance, sale, lease, or conveyance of property. The subdivision process shall implement the phasing plan.

Use Permit Approval Procedures

Sections 24-43 through 24-48.5 of the Butte County Zoning Ordinance relating to USE PERMITS shall apply to all development within the NCSP area involving land uses that are

NORTH CHICO SPECIFIC PLAN

required to obtain conditional approval.

Design Review Approval Procedures

The procedures for design review and approval shall be prepared at the direction of the Director of Development Services following approval of the NCSP.

Amendment Procedures

In accordance with the California Government Code, Sections 65453 and 65454, specific plans shall be prepared, adopted, and amended in the same manner as general plans. This Plan may be amended as necessary in the same manner as it was adopted, by ordinance. Said amendment or amendments shall not require a concurrent general plan amendment unless it is determined by County staff that the proposed amendment would substantively affect the general plan goals, policies, objectives, or programs. An initial environmental assessment shall accompany the proposed amendment, in order to determine whether the proposed amendment would require environmental assessment beyond that addressed in the NCSP EIR.

Enforcement

A violation of the NCSP constitutes a violation of the Butte County Zoning Ordinance, and shall be so administered.

CHAPTER 8 TABLES—LIST OF ASSUMPTIONS

	Acres	Dwelling Units	Average DU Size			
Residential						
SR-3	109.8	35	2,400			
SR-1	824.6	719	2,100			
R-1	233.9	912	1,750			
R-2	118.4	829	1,600			
R-3	17.1	308	700			
Total Residential Units	1,303.8	2,803				
Industrial						
	Gross Ac.	Acreage Yield	Net Ac.	Land SF	Floor/Area Ratio	Bldg. SF
M-2/OS	251.2	33.3%	83.7	3,647,059	20%	729,412
M-1	124.8	100.0%	124.8	5,436,288	20%	1,087,258
M-1/OS	115.8	33.3%	38.6	1,681,248	20%	336,250
Total Industrial	491.8		247.1	10,764,595		2,152,919
Commercial	24.6	100.0%	24.6	1,071,576	25%	267,894
Office	26.0	100.0%	26.0	1,132,560	25%	283,140
Total Non-Residential Units	542.4		297.7	12,968,731		2,703,953
City of Chico Industrial Land	162	100.0%	162.0	7,056,720	20%	1,411,344

Note	909,398	Equates to improvement cost allocated to City of Chico industrial land (see Table 8-4)
	12.7%	Percentage of above figure divided by total transportation improvement costs (see Table 8-1).

CHICO URBAN AREA



APPENDIX A

BUTTE COUNTY GENERAL PLAN POLICIES

The following is a listing of those policies from the Butte County General Plan which are relevant to the planning and decision-making of the North Chico Specific Plan. The inclusion of these policies further underscores the intended role of a specific plan to be a tool of general plan implementation — a framework upon which to interpret and fill out the larger plan's land use, infrastructure and development guidance with goals, details and financing mechanisms appropriate for the specific plan area.

The Butte County General Plan Land Use Element concludes the introduction of the policy sections with the following:

"No one policy by itself should always determine County action; decision-makers must consider all adopted policies which are relevant to a particular situation. The continual interpretation and application of policy statements to individual situations will frequently result in compromises reflecting balances and priorities among conflicting policies".

BUTTE COUNTY GENERAL PLAN LAND USE ELEMENT

B. General

Orderly Development: Government agencies, private companies and the public at large have invested substantial amounts of money in Butte County's existing system of roads, utilities and other public facilities. To maximize this investment and the level of services, utilization of existing facilities is desirable, recognizing that all designated land for development is not immediately available for use. Land owners have plans for the timing of development on their land which are to be respected.

- B.7.a. Encourage annexation to existing cities and existing districts.
- B.7.b. Promote the full utilization of sites served by existing public facilities.
- B.7.c. Encourage development in and around existing communities with public facilities.

C. Resource Management

C.1.b. Retain in an agricultural designation on the Land Use Map areas where location, natural conditions and water availability make lands well-suited to orchard and field crop use, while considering for non-agricultural use areas where urban encroachment has made inroads into agricultural areas and where past official actions have planned areas for development.

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C.1.d. Maintain minimum parcel sizes in designated agricultural areas by following comprehensive zoning principles.

C.1.e. Encourage urban expansion toward the least productive soils.

C.1.f. Allow rural residential development as a buffer between urban development and intensive crop use.

C.4.d. Require proof of adequate water supply for all new development.

C.4.e. Conservation of water and energy will be considered in approving plans for new development.

D. Residential Development

D.1.b. Provide a diversity of housing sites varying in size, density and location.

D.2.a. Correlate residential densities to soil, slope and other natural site characteristics.

D.2.b. Correlate residential densities to availability of water and sewage disposal and proximity to other public facilities.

D.2.c. Relate residential densities to intensity and compatibility of adjacent uses.

D.2.d. Balance residential densities with traffic-carrying capacities of existing and proposed circulation plans.

D.3.b. Regulate visibility, employment, advertising, parking, etc. of businesses allowed in residential areas in order to maintain a predominately residential character.

D.4.a. Allow agricultural uses and farm animals in designated residential areas where appropriate.

D.4.b. Limit density of farm animals in relation to type of animal and parcel size.

E. Economic Development

E.1.a. Provide sites and facilities to accommodate a variety of economic activities.

E.3.a. Encourage a full range of commercial services at the regional, community and neighborhood levels.

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- E.3.b. Coordinate future commercial facilities with existing and proposed transportation systems, utilities and other public facilities.
- E.3.c. Designate sufficient land for commercial facilities to fulfill needs for services and employment.
- E.3.d. Encourage the grouping of convenience and service facilities into integrated centers.
- E.3.e. Designate retail and service commercial areas in close proximity to residential development.
- E.4.a. Promote the development of new industry in the County.
- E.4.b. Locate industry near major transportation facilities which carry raw materials, finished products and commuting workers.
- E.4.c. Direct new industry to locations adequately served by major utilities and provide sufficient services and utilities to meet future industrial needs.
- E.4.e. Encourage the grouping of industrial and heavy commercial uses into integrated industrial parks.
- E.5.a. Relate the intensity and variety of commercial uses to the market accessibility of each site.
- E.5.b. Limit light commercial uses in planned industrial areas.
- E.5.c. Limit manufacturing and wholesale activities in light commercial areas.
- E.5.d. Limit residential uses in industrial and commercial areas.
- E.5.e. Encourage the separation of heavy industrial and residential areas with other uses, natural barriers or public facilities.

F. Public Facilities

- F.1.b. Provide a circulation system and plan that is consistent with and will support existing and proposed patterns and densities of land use.
- F.2.a. Encourage expansion of public water and sewer systems where development to be served conforms to adopted land use plans.
- F.3.a. Plan drainage facilities to serve areas of future urban growth.

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- F.3.b. Require adequate drainage improvements for new development.
- F.3.d. Direct future urban growth away from floodplain areas.
- F.5.b. Support development of commercial recreation facilities on suitable sites.
- F.5.c. Encourage maximum recreational use of public schools.
- F.6.b. Consider proximity to students and compatibility of adjacent uses in locating schools.
- F.6.c. Relate land use to designated school sites.
- F.7.a. Locate new fire stations with consideration to accessibility, future development and natural fire hazards.
- F.7.b. Encourage central and convenient locations for all government buildings consistent with land use plans.

G. Environmental Preservation

- G.1.a. Maintain public health and safety by requiring proper location and design for uses with offensive odors, dust, smoke, light, traffic, vibration, explosives, pollutants, insects and similar blighting influences.
- G.2.a. Consider recommended noise levels in review of proposed development.
- G.2.b. Locate noise-sensitive uses away from airports.
- G.2.c. Control locations of noisy recreational activities and events.
- G.3.a. Provide open space areas near and between designated urban areas on the Land Use Map.
- G.4.a. Protect valuable scenic areas and parks for enjoyment by residents and visitors.
- G.4.c. Encourage compatible land use patterns in scenic corridors and adjacent to scenic waterways, rivers, and creeks.
- G.5.b. Prevent development and site clearance other than river bank protection of marshes and significant riparian habitats.
- G.5.d. Regulate development to facilitate survival of identified rare or endangered plants and animals.

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G.6.a. Encourage the creation and expansion of natural and wilderness areas.

G.7.a. Identify and evaluate all cultural resources impacted by proposed projects before approval and development.

G.7.b. Preserve significant sites or require their detailed investigation by competent archaeologists.

H. Natural Hazards

H.1.a. Consider fire hazards in all land use and zoning decisions, environmental review, subdivision review and the provision of public services.

H.1.b. Guide development to areas with adequate fire protection services.

H.2.a. Consider the most recent information on seismic hazards in all zoning and subdivision decisions.

H.3.a. Limit development in areas with significant drainage and flooding problems until adequate drainage or flood control facilities are provided.

H.4.a. Correlate allowable density of development to potential for landslides, erosion and other types of land instability.

BUTTE COUNTY GENERAL PLAN CIRCULATION ELEMENT

Objective

1.2 Provide an integrated system of roads and highways that serve all land use needs.

Policies

1.2.1 Road system planning will emphasize preservation of the existing roadway network while working to increase the efficiency and capacity of the existing network.

1.2.2 The most important roads and highways should be designed and maintained to the highest possible level of service and convenience. The least important roads and highways should receive only the improvements necessary to maintain their structural integrity and operational safety. The relative importance of the County's road highway network is graphically illustrated in Figure A.

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2.1.2 Butte County will encourage and support sincere efforts by County residents to form assessment districts for road maintenance and road drainage.

Objective

2.2 Encourage development in areas that can be served by public roads in a manner that does not become an economic burden to the County, over time.

Policies

2.2.1 The short-term and long-term costs of improving and maintaining the circulation infrastructure will be a major factor in determining land use and development decisions.

2.2.3 The cost of new roads shall be borne as equitably as possible among benefiting property owners and/or users.

3.1.3 Butte County will encourage transportation modes and programs that are capable of reducing total and per capita transportation energy consumption, including: public transit, bicycle commuting, ridesharing and carpooling, and increased federal vehicle fuel efficiency standards.

Goal

4.0 Provide for a road and highway network that meets the needs of existing and anticipated movements of people and goods.

Objective

4.1 Provide for adequately designed road and street patterns to serve present and future traffic volumes.

Policies

4.1.1 For general and circulation planning purposes, the County will follow the system of classification of streets, roads, and highways as described on Section 5.0 and illustrated by Figure A.

4.1.2 Rural arterial road and highway traffic capacity levels should be planned to provide a level of service "B" and be considered to be providing acceptable service at level of service "C" when fiscal, environmental, or site constraints are prohibitive.

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4.1.3 The County will insure that arterial routes continue to serve as major traffic carriers and remain free of unnecessary future intersections, driveways, on-street parking, and traffic overloads.

4.1.4 Right-of-way needed for new roads or expansion of existing roads shall be planned for; land uses that would preclude the timely development of such right-of-way shall be prohibited.

4.1.5 The County will ensure that all road systems, including private roads, connect various properties slated for potential development, both to each other and to a publicly-maintained road system.

4.1.6 Usable road easements of adequate width shall be located as to most beneficially serve the needs of all parcels.

4.1.7 The County will assume maintenance responsibility only for roads which meet full County standards.

4.1.8 Private subdivision roads will be built to full County standards and they will be privately maintained as such throughout their maintenance cycle.

4.1.9 New roads resulting from land divisions will be constructed to County standards whenever an area has potential for significant traffic from future development. A lower standard may be considered reasonable for roads which will always serve as only lot access and will never be suitable to become County roads.

4.1.10 New land divisions should be held responsible for their fair share of the off-site road improvements needed to handle the traffic increases that they cause.

4.1.11 The County should encourage the utilization of development agreements as one way of ensuring that road development standards and plans are met.

4.1.12 The County will require erosion mitigation and control plans for new developments and for road encroachment permits to prevent soil loss during and after road development activities.

Objective

5.1 Support safety standards established by emergency and protective service agencies.

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Policies

5.1.1 All road systems, both public and private, shall provide for the safe evacuation of residents and adequate access for fire and other emergency services by providing at least two means of emergency access to an interconnected collector system.

Objective

6.1 Planning areas should be encouraged to develop with integrated, efficient, and well designed road systems.

Policies

6.1.1 Specific plans shall be encouraged for land use, circulation, and cumulative transportation impacts for planning areas where land use designations and zoning lack significant provisions to adequately project future traffic conditions and/or where common circulation needs are appropriate.

6.1.2 Specific plans developed for each planning area shall address circulation.

7.1.4. The County and its cities should develop mutual and complimentary policies regarding the timing and phasing of new urban area developments, as necessary for the logical and timely development of each urban area circulation network.

8.1.1 Urban street and highway traffic capacity levels should be planned to provide a level of service "C", and be considered to be providing acceptable service at level of service "D" when fiscal, environmental or site constraints are prohibitive.

8.1.3 Reduced street widths will be encouraged when there are practical site planning opportunities and development cost savings involved, while not jeopardizing public safety and future capacity requirements.

8.1.4 Arterial and collector streets shall be developed so as not to diminish the integrity and cohesiveness of urban neighborhoods.

8.1.5 Major residential developments should ensure adequate circulation by providing interconnecting loops and collector street patterns. Cul-de-sac and dead-end streets should be avoided on streets with more than twenty (20) units.

8.1.6 Trees located along urban streets should be preserved or replaced in the event maintenance or upgrading requires tree removal. Similar landscaping should be considered in conjunction with the development of new urban streets and parking facilities.

NORTH CHICO SPECIFIC PLAN

The County should continue working towards finding new ways to finance street tree programs, including for public and private sector contributions.

9.2.2 Developers of major traffic generating land uses shall provide fixed transit facilities such as bus shelters and pullouts, according to expected demand.

Goal

10.0 Provide for a safe and convenient bicycle transportation system which is integrated with other transportation modes.

Objective

10.1 Provide for adequate bicycle circulation and facilities as a functional alternative to the automobile, and for recreation, as funding and planning opportunities allow.

Policies

10.1.2 Construction or expansion of all major arterials shall consider bicycle paths of Class II or better.

10.1.3 Residential developments should incorporate internal circulation networks that encourage bicycle use and which connect to the external bicycle circulation system.

Objective

10.2 Provide a bicycle system which can be integrated with other transportation modes.

Policies

10.2.1 Bicycle parking facilities should be encouraged in apartment complexes, major commercial, professional office, industrial, and educational sites, along with good routes which foster bicycle use.

10.2.2 Multi-modal transportation facilities such as park-and-ride lots and bus stops, should provide adequate and secure bicycle parking facilities.

Objective

11.1 Pedestrian access should be ensured throughout urban areas.

Policies

NORTH CHICO SPECIFIC PLAN

11.1.1 Sidewalks, or their reasonable alternatives, should be provided in all urban subdivisions.

11.1.2 Handicapped access shall be incorporated into all sidewalks and other pedestrian facilities as required by State law.

11.1.3 Hiking and jogging corridors should be encouraged in urban areas, as funding and planning opportunities allow.

Objective

12.1 Provide for compatible land uses in areas that may be impacted by airport operations, so to mitigate safety and noise problems.

Policies

12.1.2 The County will ensure that land uses in the vicinity of public airports are compatible with respective airport land use plans.

13.1.3 The design and location of new development shall consider and incorporate provisions for appropriate transportation modes.

14.1.1 The County will maintain the integrity of the Chico area "greenline" adopted in 1982.

BUTTE COUNTY GENERAL PLAN NOISE ELEMENT

Objectives

- Avoid a mix of incompatible noise generating and noise-sensitive activities.
- Protect areas of the community which have "acceptable" or "sensitive" noise environments.

Policies

2. Where possible, control the sources of transportation noise to maintain acceptable levels.
3. Special consideration should be given to residential development and other noise-sensitive activities near railroads and highways.

NORTH CHICO SPECIFIC PLAN

4. Plan for airport development and discourage noise-sensitive activities near airports.
5. Control recreation activities that have the potential to cause objectionable noise.

APPENDIX B: PUBLIC FINANCE OPTIONS

The Following is a summary of many of the public financing vehicles that are currently available to fund public facilities and/or services.

Mello-Roos Community Facilities District

The Mello-Roos Community Facilities Act (the "Act") [Section 53311 et seq. of the Government Code] was enacted by the California State Legislature in 1982 to provide an alternate means of financing public infrastructure and services subsequent to the passage of Proposition 13 in 1978. The Act complies with Proposition 13, which permits cities, counties and special districts to create defined areas within their jurisdiction and, by a two-thirds vote within the defined area, impose special taxes to pay for the public improvements and services needed to serve that area. The Act defines the area subject to a special tax as a Community Facilities District (CFD).

A CFD may provide for the purchase, construction, expansion or rehabilitation of any real or other tangible property with an estimated useful life of at least five years. A CFD may also finance the costs of planning, design, engineering and consultants involved in the construction of improvements of formation of the CFD. The facilities financed by the CFD do not have to be physically located within the CFD. The facilities that can be financed by a Mello-Roos CFD include, but are not limited to, the following:

- Roads, water and sewer facilities, flood control channels.
- Local park, recreation parkway and open-space facilities.
- School sites, structures, furnishings and equipment.
- Libraries.
- Child care facilities.
- Utility improvements (limited to five percent of bond proceeds if improvements are to be taken over by a non-publicly owned utility agency).
- Any other governmental facilities which the legislative body creating the CFD is authorized by law to contribute revenue to, construct, own or operate.

A CFD may also pay for public services, including the following:

- Police protection.
- Fire protection.
- Recreation program services.
- Library services.
- Park and open space maintenance and operation.
- Removal or cleanup of hazardous substances.

A CFD, however, may only finance the services mentioned above to the extent that they are in addition to those provided in the area of CFD before the CFD was created, and may not supplant services already available within that area.

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Formation

A CFD can be established by almost any governmental or public agency, including special districts. Formation of a CFD in Butte County would be initiated by:

- A motion by the Board of Supervisors (the "Board").
- A written request signed by two member of the Board.
- A petition filed with the clerk of the Board signed by ten percent of either the registered voters residing within the proposed CFD, or ten percent of the landowners by land area within the proposed CFD.

Within the 90 days of initiating the proceedings to form the CFD, the Board would adopt a resolution of intention to establish a CFD and a resolution of necessity to incur bonded indebtedness, and determine a date for a public hearing on the formation of the CFD. The hearing must be not less than 30 days or more than 60 days from the date of the resolution of intention was adopted. At the public hearing, if the Board makes a decision to proceed with formation of the CFD, a resolution of formation, a resolution to incur bonded indebtedness, and a resolution calling for elections to authorize special taxes and the issuance of bonds, will be adopted by the Board.

If the Board decides to proceed with establishing a CFD, it must submit the levy of the special tax to the qualified electors of the proposed CFD in the next general election or in a special election to be held at least 90 days, but not more than 180 days, following the close of the public hearing. However, these time limits may be waived with the unanimous consent of the qualified electors. As required by Proposition 13, two-thirds of the voters casting ballots must support the tax if it is to be imposed. However, if there are fewer than 12 registered voters residing in the proposed district, the vote shall be by the landowners of the proposed CFD, and each landowner shall have one vote for each acre or portion of an acre of land owned within the CFD.

In many instances, the public agency, underwriter and bond counsel require that the CFD formation be validated by a California Superior Court. Validation precludes the ability of other parties to challenge any issue included in the validation hearing. A validation proceeding may delay the sale of bonds for an additional 60 to 75 days subsequent to the election.

Revenue Sources

Formation of a CFD authorizes the public agency to levy a special tax on all taxable property within the CFD, as defined in the formation documents. Property owned or irrevocably offered to a public agency may be exempted from the special tax. Mello-Roos special taxes are collected at the same time and in the same manner as regular property taxes, unless otherwise specified by the agency. Special tax revenues may be used to pay the debt service on bonds which have been sold to fund the construction or acquisition of public capital facilities, or to pay directly for facilities or public services.

There are two limitations on the amount of financing available from a CFD. The first is the

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value-to-lien ratio. "Value" is considered to be the appraised value of the property, including any entitlement and improvements in place on the date the CFD bonds are to be sold. The value is then increased to account for the value added to the property by the improvements to be constructed with funds from the bond issue.

There is no statutory requirement for a value-to-lien ratio. The standard for land secured financing in California has been a 3:1 or 4:1 ratio, depending on other factors such as the real estate market, the financial status of the developer and the overall market acceptability for land secured bonds.

The second restriction on the amount of financing available from a CFD is the total effective tax rate ("ETR") paid by a homeowner or property owner in the CFD. The ETR consists of the basic one percent ad valorem property tax levy mandated by Proposition 13, plus overrides from voter-approved bonded indebtedness and non-ad valorem taxes, assessments and parcel charges (expressed as a percentage of market value). Market value can be determined based on input from local developers, a market consultant, local realtors or an appraiser.

Again, there is no legal maximum related to this limitation, but a maximum ETR of two percent of market value has developed as a standard for residential development in many areas throughout the State. It is thought that ETRs higher than two percent may lead to market resistance by prospective homebuyers, or potential "taxpayer revolts" by overburdened homeowners. The maximum supportable ETR for a given project should also consider the maximum tax rates paid by homes in competing projects in the area and, based on the strength of the real estate market, the demand for homes in general. Commercial/industrial projects often support higher ETRs, as the property owner is able to spread the tax burden among many tenants and, therefore, is less sensitive to a higher ETR.

Special Assessment District

Introduction

In 1979, the California Court of Appeals, in the County of Fresno vs. Malmstrom, ruled that special benefit assessments levied pursuant to the Improvement Act of 1911 and Municipal Improvements Act of 1913 are not "special taxes" under Proposition 13's definition of such taxes. The result of this decision has been a proliferation of Special Assessment Districts ("AD") throughout the State, using the Municipal Improvements Act of 1913 to initiate proceedings for the formation of an AD, and the Improvement Bond Act of 1915 to issue bonds. These Acts provide mechanisms for Issuers to construct or acquire public improvements, to apportion the costs through liens against the

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properties in a designated area which directly benefit from the improvements (the "Assessment District"), and to finance the liens through the issuance of tax-exempt bonds.

Public works improvements are eligible for Assessment District financing to the extent that properties within the District receive a special, measurable, local and direct benefit from such improvements. Traditionally, improvements to be financed using an AD include, but are not limited, to streets, and roads, water, sewer, flood control facilities, utility lines and landscaping. Other types of public improvements which have a "regional" significance (e.g., major roads, bridges, flood control facilities, etc.) are only partially eligible, based on the proportion of benefit from the improvements that can be assigned to parcels within the AD. Traditionally, items of general benefit to a community, such as schools, fire stations and parks, have not been eligible for Assessment District financing.

Formation

The formation of an AD is initiated through either a petition submitted by sixty percent of the landowners in a proposed AD, or through the adoption of a Resolution of Determination and the preliminary approval of an investigative report by the County Board of Supervisors. The Board then adopts a Resolution of Intention which designates the boundaries of the proposed AD, describes the proposed improvements, orders the issuance of bonds and declares the County's intention to levy the assessments. This resolution must include an engineer's report that includes the proposed assessment diagram, which is used to determine the assessment levied against each property.

Once the Board approves the engineer's report and confirms the proposed assessments, notices are sent to all property owners within the proposed AD regarding a public hearing to be held not less than thirty days after the Board adopts the resolution of intention. At or before the public hearing, if a written protest is made by more than one-half of the property owners of the area to be assessed, no further proceedings can be taken for one year, unless the Board overrules the protest by a four-fifths vote. If no majority protests, at the conclusion of the public hearing, the Board will confirm the assessments and create a lien against each assessed parcel in the AD.

Subsequent to the confirmation of assessments, a thirty-day cash payment period is established during which any property owner can pre-pay his assessment. After this thirty-day period, bonds may be sold for all unpaid assessments in the newly-formed AD.

Revenue Sources

Each parcel of property within an AD is assessed a portion of the costs of the public improvements and services to be financed by the AD, based on the proportion of benefit received

by that parcel. Assessment liens are levied at the time of formation on the timing of the development. If a standby charge is incorporated into the program, bonds can be sold more quickly to provide funding for water-related improvements needed prior to development.

Infrastructure Financing District

Introduction

In September 1990, the Governor approved Senate Bill ("SB") 308, which provides for the formation of Infrastructure Financing Districts ("IFD") to finance regional infrastructure needs. SB 308 extends the use of tax increment ("TI") financing to underdeveloped areas within the boundaries of an IFD. IFD financing is similar to tax increment financing within redevelopment project areas, in that it uses ad valorem property tax property tax revenues to pay for public improvements without imposing special taxes or assessments on the land.

The Legislative findings with respect to SB 308 state that the Bill was adopted in response to a number of factors, including the following:

- the State and Federal governments have withdrawn in whole or in part from their former role in financing major, regional or communitywide infrastructure;
- the methods available to local agencies to finance public works often place an undue and unfair burden on buyers of new homes, especially for public works that benefit the broader community; and
- it is equitable and in the public interest to provide alternative procedures for financing public works and services needed to meet the needs of new housing and other development projects.

An IFD may finance the purchase, construction, expansion or improvement of any real or tangible property with an estimated useful life of fifteen years or longer. Authorized facilities must be of communitywide significance and provide significant benefits to an area larger than the area of the proposed IFD. Examples of permissible projects include the following:

- Highways, interchanges, arterial streets and transit facilities.
- Sewage treatment and water reclamation plants.
- Water collection and treatment facilities.
- Flood control levees and dams, retention basins and drainage channels.
- Child care facilities and libraries.
- Parks and open space.
- Facilities for the transfer and disposal of solid waste.

Formation

Proceedings to establish an IFD are initiated by the County Board of Supervisors through the adoption of a Resolution of Intention. The resolution must identify the boundaries of the IFD, the

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type of facilities to be financed and a time and place for a public hearing. Following adoption of the Resolution of Intention, the Board must direct the appropriate official to prepare an Infrastructure Financing Plan to include a map of the proposed boundaries of the IFD, the location, timing and costs of all public facilities (whether or not to be provided by the IFD) required to serve the proposed development, and a financing section.

The financing section should include all of the following:

- The maximum portion of TI revenue from each affected taxing entity proposed to be committed to the IFD for each year.
- The TI revenues projected to be received by the IFD in each year of the Plan.
- The amount of debt intended to be incurred by the IFD.
- The maximum amount of TI revenue the IFD may receive.
- An analysis, for each affected taxing entity, of the costs and the taxes, fee, and other revenues to be received.
- A fiscal impact analysis of the IFD, and the development within the IFD, on each of the affected taxing entities.
- A date on which the IFD will cease to exist (maximum term of 30 years).

A copy of the Plan is sent to each of the affected taxing entities and to the property owners within the proposed IFD. A public hearing on the Plan is held no sooner than 60 days after the Plan is sent to all affected taxing entities. The Board may not adopt a resolution proposing formation of the IFD unless a resolution approving the Plan has been adopted by all of the affected taxing entities prior to the hearing. The Plan may be amended to exclude any affected agency that does not approve the Plan, or to amend the percentage of increment from an agency that will be passed through to the IFD.

Subsequent to the public hearing, if the Board adopts a resolution proposing adoption of the Plan and formation of the IFD, an election must be called to submit the proposal to the qualified electors. The election must be held at the next general or special election, at least 90 day, but no more than 180 days, following adoption of the resolution. This time limit may be waived with the unanimous consent of the qualified electors.

If there are twelve or more registered voters, the electors will be all registered voters within the IFD; otherwise, the qualified electors will be landowners within the IFD, with each landowner receiving one vote for each acre of property owned. If two-thirds of the votes cast are in favor of forming the IFD, the Board will adopt an ordinance creating the IFD and adopting the Plan.

Revenue Sources

As discussed above, SB 308 extends tax increment financing to undeveloped areas within an IFD to allow financing of major, regional improvements without additional taxes and assessments being levied on property owners. When forming an IFD, a "base year" is identified; the ad valorem tax revenues paid on the assessed property value in the base year continue to be passed through to the agencies that receive ad valorem tax revenues. However, the revenues that result from an increase in assessed value above the base year assessed value are called tax increment ("TI") revenues, and a portion of these revenues are retained by the IFD. An IFD can then issue bonds secured by the TI revenue it receives, or will receive, because of the continuing increase in assessed value.

The TI revenues available from an IFD are limited by the following:

- TI revenues to school districts, community college districts and county offices of education may not be allocated to an IFD.
- TI revenues to other agencies may be allocated to an IFD only if each agency approves the Plan by resolution. Agencies which fail to approve the Plan must be excluded from allocation of TI revenues.

Revenues from an IFD may be pledged to repay bonds issued by a Special Assessment District or Mello-Roos CFD, or may be advanced to an Integrated Financing District as part of a reimbursement agreement. The IFD may issue tax allocation bonds or Certificates of Participation payable from anticipated tax increment. Bonds sold by an IFD must be sold at a public sale.

Integrated Financing District

Introduction

The Integrated Financing District Act (the "Act") was designed to achieve overall equity among landowners by providing a formal mechanism to reimburse the initial developers (or public agency) for the up-front funding of infrastructure needed prior to development. The Act creates the ability for local governmental entities to levy contingent assessments on owners of land within special districts which are created to levy noncontingent assessments or special taxes for the financing of public facilities (i.e., Mello-Roos or Special Assessment Districts). An Integrated Financing District (IGFD) can be used to finance any improvements authorized in the legislation of any co-financing district, including major regional improvements and local community facilities, such as schools and fire stations.

In its most likely use, an IGFD is formed over an area that benefits from improvements being financed through another financing mechanism. The benefit received by each parcel is determined and a "contingent lien" is placed on all parcels that benefit from the improvement, but have not paid

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for its construction. A warrant is issued, in the amount of the contingent lien on a lien parcel, to the party that will ultimately receive the reimbursements. Once development begins on the parcel, the contingent lien will become due and payable to the party who holds a "warrant" securing the lien.

Formation

An IGFD is created in the same proceedings to create the co-financing district, if the following items are incorporated in the resolution of intention to form the districts:

- A description of the boundaries of the IGFD, which need not be the same as the boundaries of an existing Assessment District or Mello-Roos District.
- A description of the rate and method of apportionment over time of any type of levy proposed within the IGFD, and the contingency under which each levy will be made. The contingent assessments will be identified as a fixed dollar amount per parcel, which may be adjusted annually thereafter.
- A description and estimated cost of any facilities to be constructed with funds contributed from the IGFD.
- A description of the proposed reimbursement agreement

Notice of a public hearing shall be made in the same manner as required for the co-financing district being used in conjunction with the IGFD. If more than one-half of the area of the property within the proposed IGFD protests against the proposed contingent assessments, no further proceedings shall be taken for the period of one year. Without a majority protest, the Board may abandon the proposed establishment of the IGFD, or combine the co-financing district with the district to levy contingent liens, to create the Integrated Financing District.

Revenue Sources

An IGFD issues securities in the form of warrants which represent interest in a reimbursement fund into which contingent assessments are paid. The contingent assessment is a lien against property which is not levied until the occurrence of a certain event that is associated with land development, such as the approval of a tentative or vesting tentative subdivision map, a zoning change, or the issuance of a building permit.

The assessments must be fixed dollar amount which may be adjusted annually on a percentage basis as determined by the Board. Assessments are levied on an acreage basis, with allowance for different rates for various land uses. The contingent assessment may be levied instead of, in conjunction with, or in addition to a non-contingent assessment or special tax and may be collected in annual installments or in lump sum payments similar to a fee program. The combined levy of a

contingent and non-contingent assessment and/or special tax should result in an amount paid by a parcel that represents the proportion of benefit received by that parcel.

The revenue potential from an IGF is also limited by a value-to-licen ratio, in that the contingent assessment, whether collected or not, represents an obligation of the parcel. If the contingent assessment is not required to be paid in one limp-sum, it may ultimately be passed on to future homebuyers or purchasers of commercial/industrial property. If this is the case, the contingent assessment should be considered as part of the total effective tax rate.

Certificates of Participation

Introduction

Certificates of Participation ("COPs") provide long-term financing for public improvements via a lease or installment sales structure. COPs permit the acquisition or construction of specific equipment, land or facilities through the occurrence of debt, without requiring a local election. Although the structure of COPs sound complicated, it is actually an efficient and straight-forward method of securing tax exempt financing for public facilities by taking advantage of an available stream of revenues.

The principal parties to a COPs financing include a public agency, a non-profit corporation and a trustee. The non-profit corporation may be formed specifically to construct necessary improvements, the funds for which are generated from the procceds of the COPs sale. The non-profit corporation may also be an existing agency, such as a redevelopment agency, a joint powers authority or an economic development corporation. However, the actual responsibilities for construction are generally delegated to the public agency. The non-profit corporation then leases or sells the land and facilities back to the public agency in return for lease or installment sales payment.

The investors who purchased the COPs receive a specified portion of the public agency's payment as payment of the principal and interest due on their COPs. The Certificates are secured by the by the public agency's pledge to make appropriations from its general fund (or other special fund) to cover its lease or installment sales payments. The trustee is responsible for accepting those payments and then disbursing them to the Certificate holders.

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PRELIMINARY

The issuance of COPs does not require the formation of a special district. COPs are strictly a type of debt issuance and, therefore involve only the parties involved in putting together the transaction. The players typically include an underwriter, a bond counsel, a trustee and, if necessary, a financial consultant to evaluate the expected stream of revenues.

A COPs issue requires a series of legal documents as well. First, a lease or installment sales agreement has to be drafted between the County and the non-profit corporation. Second, documentation is necessary to specify the procedures for issuance and payment of the COPs, and to regulate the disbursement of COPs proceeds to finance the proposed facilities.

Finally, agreements are required to empower the County to oversee construction and to obligate the trustees to receive lease or installment sales payments for disbursement to Certificate holders. Issuance of COPs is authorized by approval of a resolution by the Board of Supervisors.

Sources of Funds

COPs are secured by the covenant of the County to make annual appropriations in an amount sufficient to service the certificates. The appropriations may come from the County general fund or from a designated special fund, such as an enterprise fund for sewer and water services for installment payments received from a private project. If the facility being financed by the COPs is revenue-producing, those revenues may also be used to make lease payments.

The revenue potential of COPs is limited by the availability of revenues which may be appropriated each year to make lease payments. Since the passage of the Gann Amendment in 1979, annual appropriations of government agencies, including County government, have been limited to prior year appropriations adjusted for changes in the cost of living and population. In a period of declining transfer payments from the Federal and State levels to County and local governments, the availability of funds is further limited.

County Services Area

Introduction

County Service Areas ("CSA") are designed to provide a mechanism to furnish extended public services to unincorporated areas experiencing high growth. A CSA is authorized to provide any governmental services which the County is authorized by law to perform and which the County

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does not already perform to the same extent on a countywide basis. The costs, then, are borne by the citizens who directly benefit from the increased services, while the decision-making remains with the County Board of Supervisors.

Formation

Formation of a CSA can be initiated in three ways:

- A petition signed by ten percent of the registered voters residing within the proposed area
- A written request by two members of the Board of Supervisors or a resolution adopted by majority vote
- A resolution adopted by the city council of an incorporated city within the County

Once initiated, formation of the CSA requires application to and approval of the LAFCO. A CSA is not a special district, and therefore does not require majority approval in a registered voter election.

Revenue Sources

Services provided through a CSA can be funded through user fees or special assessments. A user fee must directly relate to the cost of providing such services and the benefit received by each user; the revenues from this source are strictly limited. There is no statutory limit on the amount of special assessment levied by a CSA, but those assessments would be levied on the property tax bill and would therefore be considered as part of the total effective tax rate.

Impact Fees

Introduction

Impact fees are monetary exactions (other than taxes or special assessments) that are charged by local agencies in conjunction with approval of a development project. Impact fees are levied for the purpose of defraying all or a portion of the costs of any public facility, improvement or amenity which benefits the project.

Formation

The collection of impact fees does not require formation of a special district.

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Revenue Sources

Impact fees are paid by builders or developers, typically at the time a building permit is issued. The public facilities funded by impact fees must be specifically identified, and there must be a reasonable relationship, or "nexus", between the type of development project and the need for the facilities, the cost of the facilities and the need to impose a fee.

While developer fees cannot typically be leveraged (i.e., provide security for bonds or other debt instruments), fees can be used in conjunction with debt financing to help retire bonds secured by other means (e.g., land). In this case, developer fees can generate supplemental revenues to reduce future special taxes or assessments, or free up tax increment or other revenues for alternative uses. Developer fees can also be used to generate reimbursement revenues to property owners or public agencies who have previously paid more than their fair share of public improvement costs.

Landscaping and Lighting District

The Landscaping and Lighting Act of 1972 permits the installation, maintenance and servicing of landscaping and lighting through annual assessments on real property benefiting from the improvements. The act also permits construction and maintenance of appurtenant features including curbs, gutters, walls, sidewalks or paving, and irrigation or drainage facilities.

One or more Landscaping and Lighting District may be formed to fund the maintenance of roadway medians and street trees, street lights, and parks. Park maintenance costs over the fair share cost of park maintenance standards provided by the City General Fund will be funded through the Landscaping and Lighting District. The District may also be used to fund the maintenance of the retention basin landscaping, as well as certain landscaping improvements if it is deemed by the City as the most cost-effective method.

APPENDIX C:

SUMMARY OF STORM DRAINAGE COSTS

PRELIMINARY STORM DRAINAGE COST OPINIONS

The following costs are in November, 1992 dollars (San Francisco Construction Cost Index = 6292). The costs include 15% for engineering design and administration and a 20% total project contingency.

**TABLE T13
PRELIMINARY COST OPINIONS
NCSP STORM DRAINAGE TRUNK COLLECTION SYSTEM**

Description	Unit	Unit Cost	Quantity	Cost
24" Storm Drain Line	LF	46.00	4530	\$208,380
30" Storm Drain Line	LF	50.00	2190	109,500
36" Storm Drain Line	LF	55.00	1070	58,850
Storm Drain Manholes	EA	1750.00	20	35,000
Detention Basins w/ Pumps				
Basin #1	JOB	130,350.00	1	130,350
Basin #2	JOB	94,200.00	1	94,200
Basin #3	JOB	108,700.00	1	108,700
Basin #4	JOB	77,650.00	1	77,650
Subtotal				\$822,630
Engineering and Administration @ 15%				123,395
Subtotal				\$946,025
Project Contingency @ 20%				189,205
Total				\$1,135,230

APPENDIX D: SUMMARY OF SANITARY SEWER COSTS

PRELIMINARY SEWER COST OPINIONS

The following costs are in November, 1992 dollars (San Francisco Construction Cost Index = 6292). The costs include 15% for engineering design and administration and a 20% total project contingency.

**TABLE T14
PRELIMINARY COST OPINIONS
NCSP SEWER TRUNK COLLECTION SYSTEM**

Description	Unit Cost	Quantity	Cost
10-inch sewer line	60	3,450 lf	\$207,000
12-inch sewer line	70	4,000 lf	280,000
15-inch sewer line	80	3,500 lf	280,000
18-inch sewer line	90	4,450 lf	400,500
Subtotal			\$1,167,500
Engineering and Administration at 15%			175,000
Subtotal			\$1,342,500
Project Contingency at 20%			268,500
Total			\$1,611,000

NORTH CHICO SPECIFIC PLAN

Key Assumption:

The North Chico Specific Plan Area will generate 2.68 mgd of the 5.1 mgd flowing through the off-site sewer outfall at buildout. Therefore, based on flow generations, the North Chico Specific Plan is responsible for approximately 50% of the outfall cost.

**TABLE T15
PRELIMINARY COST OPINIONS
SEWER TRUNK OUTFALL TO REGIONAL PLANT (OFF-SITE)**

Outfall to Plant			
Description	Quantity	Unit Cost	Cost
33-inch sewer line	26,650 lf	150	\$3,997,500
27-inch sewer line	7,950 lf	130	1,033,500
24-inch sewer line	3,250 lf	120	390,000
21-inch sewer line	1,600 lf	100	160,000
Subtotal			\$5,581,000
Engineering and Administration at 15%			837,000
Subtotal			\$6,418,000
Project Contingency at 20%			1,284,000
Total			\$7,702,000
North Chico Specific Plan Responsibility at 50%			\$3,851,000
Interim			
*24-inch sewer line	900 lf	120	\$108,000
*21-inch sewer line	1,600 lf	100	160,000
5.0 mgd pump station	LS		700,000
Force main to NECSAD Line	LS		500,000
Subtotal			\$1,468,000
Engineering and Administration at 15%			220,000
Subtotal			\$1,688,000
Project Contingency at 20%			338,000
Total			\$2,026,000
North Chico Specific Plan Responsibility at 50%			\$1,013,000
<i>*Facilities built as a part of ultimate facilities.</i>			

**TABLE 8-3
SPREAD OF SEWER ALTERNATIVE #2 FEES**

Land Use	Acreage	ADDWF Acre Per Day	Total Avg		Spread of Costs by Use	Number of DU or SF	Approx. Cost Per DU or SF
			Dry Weather Flow (Gal.)	% Of Total			
R-1	233.9	1,150	268,985	28.9%	\$892,838	912	\$979
R-2	118.4	1,300	153,920	16.5%	\$510,905	829	\$616
R-3	17.1	2,900	49,590	5.3%	\$164,603	308	\$534
Industrial	247.1	1,500	370,682	39.8%	\$1,230,399	2,153,000	\$0.57
Commercial	24.6	1,500	36,900	4.0%	\$122,482	268,000	\$0.46
Office	26.0	1,500	39,000	4.2%	\$129,452	283,000	\$0.46
School	8.5	1,500	12,750	1.4%	\$42,321	n/a	n/a
Total			931,827	100.0%	\$3,093,000		

**TABLE 8-4
SPREAD OF TRANSPORTATION FACILITIES BASED ON TRIP GENERATION**

Land Use	Unit Number	Units	Trip Factor Per Unit	Daily Trips Per Unit	% Of Total	Spread of Costs by Use	Number of DU or SF	Approx. Cost Per DU or SF
SR-1	719	Dwelling Units	9.55	6,866	10.4%	744,118	719	\$1,035
R-1	912	Dwelling Units	9.55	8,710	13.2%	943,860	912	\$1,035
R-2	829	Dwelling Units	9.55	7,917	12.0%	857,960	829	\$1,035
R-3	308	Dwelling Units	6.47	1,993	3.0%	215,956	308	\$701
Industrial	247	Acres	51.80	12,801	19.4%	1,387,231	2,153,000	\$0.64
Industrial	162	Acres	51.80	8,392	12.7%	909,398	1,411,000	\$0.64
Commercial	268	Thousand SF	54.50	14,600	22.1%	1,582,227	268,000	\$5.90
Office	283	Thousand SF	14.03	3,972	6.0%	430,495	283,000	\$1.52
School	30.0	Employees	13.39	402	0.6%	43,532	n/a	
Total				65,987	100.0%	\$7,151,000		

**TABLE 8-2 (Schedules 5-7)
IMPROVEMENT COST DETAIL & ALLOCATION AMONG LAND USES**

Schedule 5. Neighborhood Parks (Costs)			
	Acres	\$/Acre	Extension
Land Acquisition	15	\$25,000	\$375,000
Capital Improvements	15	\$75,000	1,125,000
Community Building (5,000 sf)			600,000
Total Neighborhood Park Facilities Cost			\$2,100,000

Schedule 5. Chico Area Recreation District (CARD) Park Fee			
Land Use	DU or SF	Fee Per Unit	Extension
SR-3	35	\$2,126	\$74,410
SR-1	719	\$2,126	1,528,594
R-1	912	\$2,126	1,938,912
R-2	829	\$2,126	1,762,454
R-3	308	\$1,799	554,092
Industrial	2,153,000	\$0.01	21,530
Commercial	268,000	\$0.02	5,360
Office	283,000	\$0.01	2,830
Total Chico Area Recreation District (CARD) Park Fee			\$5,888,182

Schedule 6. Chico Unified School District (CUSD) Impact Fee					
Land Use	Avg. DU Size	Fee/SF	Fee Per Unit	# DU	Extension
SR-3	2,400	\$1.84	\$4,416	35	\$154,560
SR-1	2,100	\$1.84	\$3,864	719	2,778,216
R-1	1,750	\$4.60	\$8,050	912	7,341,600
R-2	1,600	\$4.60	\$7,360	829	6,101,440
R-3	700	\$4.60	\$3,220	308	991,760
Total CUA Street Facilities Fee					\$17,367,576

Schedule 7. Butte County Sheriff Fees					
Land Use	DU or SF	Fee Unit	Fee Per Unit	Extension	
SR-3	35	Dwelling Unit	\$360	\$12,600	
SR-1	719	Dwelling Unit	\$360	258,840	
R-1	912	Dwelling Unit	\$360	328,320	
R-2	829	Dwelling Unit	\$360	298,440	
R-3	308	Dwelling Unit	\$252	77,616	
Industrial	2,153,000	Square Feet	\$0.03	64,590	
Commercial	268,000	Square Feet	\$0.03	8,040	
Office	283,000	Square Feet	\$0.03	8,490	
Total				\$1,056,936	

**TABLE 8-2 (Schedules 1-4)
IMPROVEMENT COST DETAIL & ALLOCATION AMONG LAND USES**

Schedule 1. Chico Urban Area (CUA) Street Facilities Fee			
Land Use	DU or SF	Fee Per Unit	Extension
SR-3	35	\$1,331	\$46,585
SR-1	719	\$1,331	956,989
R-1	912	\$1,331	1,213,872
R-2	829	\$804	666,516
R-3	308	\$804	247,632
Industrial	2,153,000	\$0.10	215,300
Commercial	268,000	\$1.75	469,000
Office	283,000	\$0.29	82,070
Total CUA Street Facilities Fee			\$3,897,964

Schedule 2. Chico Urban Area Sewer Trunk Line Capacity Fees				
Land Use	DU or Acres	Fee Unit	Fee Per Unit	Extension
R-1	912	Dwelling Unit	\$977	\$891,024
R-2	829	Dwelling Unit	\$977	809,933
R-3	308	Dwelling Unit	\$619	190,652
Industrial	247.1	Acre	\$4,418	1,091,781
Commercial	24.6	Acre	\$4,418	108,683
Office	26.0	Acre	\$4,418	114,868
Total				\$3,206,941

Schedule 3. Chico Urban Area Sewer Treatment Plant Fees				
Land Use	DU or Acres	Fee Unit	Fee Per Unit	Extension
R-1	912	Dwelling Unit	\$824	\$751,488
R-2	829	Dwelling Unit	\$824	683,096
R-3	308	Dwelling Unit	\$522	160,776
Industrial	247.1	Acre	\$3,725	920,526
Commercial	24.6	Acre	\$3,725	91,635
Office	26.0	Acre	\$3,725	96,850
Total				\$2,704,371

Schedule 4. Trail System				
	LF-Unit	\$/LF-Unit	Extension	
Path of Trail (LF*\$/LF)	34,000	\$6.00	\$204,000	
Off Street Path (LF*\$/LF)	18,000	\$8.50	153,000	
Three Foot/Bike Bridges	3.00	\$20,000	60,000	
<i>Sub-Total Trail System Direct Costs</i>			\$417,000	
Construction Contingency	20%		\$83,400	
Engineering & Administration	15%		62,550	
Total Trail System			\$562,950	

CHAPTER 8 TABLES—LIST OF ASSUMPTIONS

<i>Residential</i>			
	Acres	Dwelling Units	Average DU Size
SR-3	109.8	35	2,400
SR-1	824.6	719	2,100
R-1	233.9	912	1,750
R-2	118.4	829	1,600
R-3	17.1	308	700
Total Residential Units	1,303.8	2,803	

<i>Industrial</i>						
	Gross Ac.	Acreage Yield	Net Ac.	Land SF	Floor/Area Ratio	Bldg. SF
M-2/OS	251.2	33.3%	83.7	3,647,059	20%	729,412
M-1	124.8	100.0%	124.8	5,436,288	20%	1,087,258
M-1/OS	115.8	33.3%	38.6	1,681,248	20%	336,250
Total Industrial	491.8		247.1	10,764,595		2,152,919

Commercial	24.6	100.0%	24.6	1,071,576	25%	267,894
Office	26.0	100.0%	26.0	1,132,560	25%	283,140
Total Non-Residential Units	542.4		297.7	12,968,731		2,703,953

City of Chico Industrial Land	162	100.0%	162.0	7,056,720	20%	1,411,344
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Note	909,398	Equates to improvement cost allocated to City of Chico industrial land (see Table 8-4)
	12.7%	Percentage of above figure divided by total transportation improvement costs (see Table 8-1)

**TABLE 8-1
SUMMARY OF CAPITAL IMPROVEMENTS & COSTS**

Facility Description	Total Estimated Costs	Source
Transportation		
<i>Hicks Lane Realignment & Widening</i>		
Acquisition of Right of Way	\$2,200,000	
Construction	207,000	
Hicks Road Bridge Replacement at Mud Creek	1,129,000	\$3,536,000
<i>New Arterial Road</i>		
Hicks Lane to Garner	2,705,000	
Garner to SR 99	524,000	\$3,229,000
<i>Intersections</i>		
SR 99/Keefe	45,000	
Eaton/SR 99 (SB Ramps)	126,000	
Eaton/SR 99 (NB Ramps)	215,000	\$386,000
Sub-Total Transportation Improvements	\$7,151,000	Table 8-4
<i>Plus Chico Urban Area Street Facilities Fee</i>	\$3,898,000	Table 8-2, Sched. 1
Subtotal Transportation Facilities	\$11,049,000	
Sanitary Sewer		
<i>Alternative 1: City of Chico</i>		
On-Site Main Lines	\$1,611,000	
Trunk Line To Lift Station	1,047,000	
City Trunk Line Fees	3,207,000	Table 8-2, Sched. 2
City Sewer Fees (WPCP Capacity Fee)	2,704,000	Table 8-2, Sched. 3
Total Sewer Alternative #1	\$8,569,000	
<i>Alternative 2: County</i>		
On-site Main Lines	\$1,611,000	
Treatment Plant, Land, Trunk Line	6,628,000	
Total Sewer Alternative #2	\$8,239,000	
Other Infrastructure/Systems		
Fire Station	Cost estimate; allocated among land uses.	\$1,500,000 Table 8-5
Storm Drainage	Cost estimate; allocated among land uses.	\$1,522,000 Table 8-6
Trail System	Cost estimate; allocated per dwelling unit.	\$563,000 Table 8-2, Sched. 4
Parks (CARD Fee)	Figure derived by existing CARD fee structure.	\$5,888,000 Table 8-2, Sched. 5
Schools	Figure derived by existing CUSD fee structure.	\$17,368,000 Table 8-2, Sched. 6
Sheriff	Figure derived by existing County fee structure.	\$1,057,000 Table 8-2, Sched. 7
Total Other Infrastructure/Systems		\$27,898,000
Total w/ Sanitary Sewer Alternative 1		\$47,516,000
Total w/ Sanitary Sewer Alternative 2		\$47,186,000

**TABLE 8-8
NORTH CHICO SPECIFIC PLAN FACILITY COST AND FUNDING SUMMARY**

Infrastructure Type	Total Estimated Cost	County/CARD Fees	NCSF Impact Fees	School Fees	City Sewer Fees	Mello-Roos or Assess. Dist.	City Contribution
Transportation Facilities							
Hicks Lane Realignment & Widening	\$3,536,000		\$3,086,000				\$450,000
New Arterial Road Intersections	3,229,000		2,818,000				411,000
	386,000		337,000				49,000
Sub-Total Improvement Costs	\$7,151,000		\$6,241,000				\$910,000
Plus Chico Urban Area Street Facilities Fee	3,898,000	\$3,898,000					
Sub-Total Transportation Facilities	\$11,049,000	\$3,898,000	\$6,241,000				\$910,000
Sanitary Sewer							
Alternative 1: City of Chico							
On-Site Main Lines	\$1,611,000					\$1,611,000	
Trunk Line To Lift Station	1,047,000					1,047,000	
City Trunk Line Fees	3,207,000				3,207,000		
City Sewer Fees (WPCP Capacity Fee)	2,704,000				2,704,000		
Total Sewer Alternative #1	\$8,569,000				\$5,911,000	\$2,658,000	
Alternative 2: County							
On-site Main Lines	\$1,611,000		\$815,000			\$796,000	
Treatment Plant, Land, Trunk Line	6,628,000		2,278,000			4,350,000	
Total Sewer Alternative #2	\$8,239,000		\$3,093,000			\$5,146,000	
Fire Station	\$1,500,000		\$1,500,000				
Storm Drainage Facilities	\$1,522,000		\$1,522,000				
Trail System	\$563,000		\$563,000				
Parks (CARD Fee)	\$5,888,000	\$5,888,000					
Schools	\$17,368,000			\$17,368,000			
Sheriff	\$1,057,000	\$1,057,000					
Total w/ Sanitary Sewer Alternative 1	\$47,516,000	\$10,843,000	\$9,826,000	\$17,368,000	\$5,911,000	\$2,658,000	\$910,000
Total w/ Sanitary Sewer Alternative 2	\$47,186,000	\$10,843,000	\$12,919,000	\$17,368,000		\$5,146,000	\$910,000

**TABLE 8-7
NORTH CHICO SPECIFIC PLAN FEE SUMMARY**

Land Use	Acres	DU or SF	Chico Street Facilities	Chico Sewer Trunkline	Chico Treatment Plant	Trails System	Parks (CARD)	School (CUSD)	Sheriff (County)	Roads & Bridges	Storm Drainage	Fire Station	Total w/ Sewer Alt.#1
Table Reference			Tbl. 8-2(1)	Tbl. 8-2(2)	Tbl. 8-2(3)	Tbl. 8-2(4)	Tbl. 8-2(5)	Tbl. 8-2(6)	Tbl. 8-2(7)	Tbl. 8-4	Tbl. 8-5	Tbl. 8-6	
<i>Residential</i>													
SR-3	109.8	35	1,331	n/a	n/a	201	2,126	4,416	360	1,035	343	493	\$10,305
SR-1	824.6	719	1,331	n/a	n/a	201	2,126	3,864	360	1,035	401	493	\$9,810
R-1	233.9	912	1,331	977	824	201	2,126	8,050	360	1,035	440	493	\$15,837
R-2	118.4	829	804	977	824	201	2,126	7,360	360	1,035	251	411	\$14,349
R-3	17.1	308	804	619	522	201	1,799	3,220	252	701	103	370	\$8,591
<i>Total Residential Units</i>	<i>1,303.8</i>	<i>2,803</i>											
<i>Industrial/Commercial</i>													
Industrial	247.1	2,153,000	0.10	0.51	0.43	n/a	0.01	n/a	0.03	0.64	0.22	0.07	\$2.01
Commercial	24.6	268,000	1.75	0.41	0.34	n/a	0.02	n/a	0.03	5.90	0.18	0.11	\$8.74
Office	26.0	283,000	0.29	0.41	0.34	n/a	0.01	n/a	0.03	1.52	0.18	0.18	\$2.96
<i>Total Industrial/Commercial</i>	<i>271.7</i>	<i>2,421,000</i>											

NORTH CHICO SPECIFIC PLAN

**TABLE T16
PRELIMINARY COST OPINIONS NCSP TREATMENT FEES**

Type	Fee	Quantity	Total
Residential Unit	\$1,000/unit	4,465	\$4,465,000
Commercial/Ind.	4,400/acre	264	1,161,600
Total			\$5,626,600

**TABLE T17
PRELIMINARY COST OPINIONS
PACKAGED WASTEWATER TREATMENT PLANT**

Description	Quantity	Unit Cost	Cost
Site Improvement	1 LS	400,000	\$4,465,000
Package Treatment Plant	1.1 mgd	2/gal	2,200,000
Effluent Pump Station and Outfall	LS	800,000	800,000
Subtotal			\$3,400,000
Engineering and Administration @ 15%			510,000
Subtotal			\$3,910,000
Plant Site	15 Ac	50,000/acre	750,000
Subtotal			\$4,660,000
Project Contingency @ 20%			932,000
*Total			\$5,592,000

* Packaged plant cost for North Chico Specific Plan Area only. There will be some economy of scale if other areas are added.

**TABLE 8-5
SPREAD OF STORM DRAINAGE COSTS**

Land Use	Acreage	SCS Curve No.	Acreage Curve No.	% Of Total	Spread of Costs by Use	Number of DU or SF	Approx. Cost Per DU or SF
SR-3	109.8				\$12,000	35	\$343
SR-1	824.6				\$288,000	719	\$401
R-1	233.9	83	19,414	32.9%	\$401,685	912	\$440
R-2	118.4	85	10,064	17.0%	\$208,232	829	\$251
R-3	17.1	90	1,539	2.6%	\$31,843	308	\$103
Industrial	247.1	91	22,488	38.1%	\$465,295	2,153,000	\$0.22
Commercial	24.6	94	2,312	3.9%	\$47,845	268,000	\$0.18
Office	26.0	94	2,444	4.1%	\$50,568	283,000	\$0.18
School	8.5	94	799	1.4%	\$16,532	n/a	\$1,945
Total			59,060	100.0%	\$1,522,000		

**TABLE 8-6
SPREAD OF FIRE STATION COSTS**

Land Use	Acres	Units DU or SF	Res./DU or SF/Employee	Residents or Employees	% Population By Land Use	Cost By Land Use	Cost Per DU or SF
SR-3	109.8	35	3.00	105	1.1%	\$17,094	\$493
SR-1	824.6	719	3.00	2,157	23.4%	\$351,169	\$493
R-1	233.9	912	3.00	2,736	29.7%	\$445,433	\$493
R-2	118.4	829	2.50	2,073	22.5%	\$337,412	\$411
R-3	17.1	308	2.25	693	7.5%	\$112,824	\$370
Industrial	247.1	2,153,000	750	957	10.4%	\$155,786	\$0.07
Commercial	24.6	268,000	500	179	1.9%	\$29,088	\$0.11
Office	26.0	283,000	300	314	3.4%	\$51,193	\$0.18
Total All Land Uses				9,214	100.0%	\$1,500,000	

Note: Non-residential uses assume 1/3 of employee (8-hour work day) to calculate population, e.g. 2,153,000sf divided by 750 employees/sf divided by 1/3 (work day) equals 957.



Resolution No. 95-47

**A RESOLUTION OF THE BUTTE COUNTY BOARD OF
SUPERVISORS AMENDING THE BUTTE COUNTY GENERAL
PLAN AND ADOPTING THE NORTH CHICO SPECIFIC PLAN**

WHEREAS, Butte County initiated the specific planning process for the land area generally known as CSA 87 in the north portion of Chico; and

WHEREAS, numerous informational and discussion meetings were held by the consultants and representatives of the County with the property owners in CSA 87 and with representatives of the City of Chico on many subjects pertaining to the North Chico Specific Plan, Rezone, General Plan Amendment, including but not limited to, environmental constraints, growth pressures, circulation, drainage, airport compatibility; and

WHEREAS, a Draft Environmental Impact Report (DEIR) was prepared and circulated for public comment and the Planning Commission extended the public comment period for an additional thirty days beyond the statutorily required thirty days; and

WHEREAS, the Specific Plan text and accompanying maps were prepared with reference to and in consideration of the 1983 *Airport Land Use Handbook* and, when they became available, the 1993 *Draft Airport Land Use Handbook* and the *Final Airport Land Use Handbook*; and

WHEREAS, the Specific Plan text and accompanying maps were prepared with reference to and in consideration of the F.A.R. Part 150 Noise Exposure Map and related reports drafted in connection with the City of Chico Airport Environs Plan update process and consultations regarding land use compatibility conducted with the City of Chico's airport planning consultant; and

WHEREAS, the Butte County Planning Commission held duly advertised and noticed public hearings starting on January 13, 1994 and concluding on November 16, 1994 with a recommendation to adopt the Rezone, General Plan Amendment, Specific Plan and EIR; and

WHEREAS, the Butte County Board of Supervisors held a duly advertised and noticed public hearing on January 10, 1995 to consider the Planning Commission's recommendations regarding the Rezone, General Plan Amendment, Specific Plan and

EIR and the action of the Butte County Airport Land Use Commission's finding of inconsistency with the 1978 Airport Land Use Plan; and

WHEREAS, public hearings at both the Butte County Planning Commission and the Butte County Board of Supervisors considered the issue of overriding the findings of inconsistency with the 1978 adopted Airport Land Use Plan for Chico Municipal Airport; and

WHEREAS, substantial testimony has been heard both pro and con, and changes to the Plan have been made as a result of testimony and public review of the Specific Plan and EIR;

NOW THEREFORE BE IT RESOLVED THAT the Butte County Board of Supervisors do hereby find and adopt the following:

Section 1: Environmental Findings.

- A. An Environmental Impact Report (EIR) has been prepared in compliance with the California Environmental Quality Act; and
- B. Comments on the Draft EIR were solicited, received, adequately addressed and incorporated into the Final EIR; and
- C. Changes, alterations or mitigation measures listed in Section 3 of the Final EIR have been required or incorporated into the project thereby eliminating or substantially lessening significant effects identified by the County, responsible agencies and members of the public; and
- D. The Board of Supervisors has independently reviewed, analyzed and considered the EIR and finds that the EIR reflects the independent judgement of the County of Butte; and
- E. Implementation of the Specific Plan will result in unavoidable and adverse impacts in which no mitigation is available other than implementation of the No-Project/No Development Alternative. These significant unavoidable impacts include traffic and circulation, air quality impacts and land use impacts. However, benefits discussed in 1, 2, and 3 below override these significant adverse impacts and the Board of Supervisors makes the following Statement of Overriding Considerations:
 1. Traffic and Circulation Impacts: As discussed on Pages 9-9 of the Draft EIR the impacts of the project were examined based upon the assumption that no new traffic signals would be installed along State Route 99 (per Caltrans policy) and that existing lane configurations would be used as a basis for all scenarios. In addition, the New Arterial

intersection is proposed to remain as an at grade intersection of State Route 99. As stated on Page 2-2 of the Final EIR (FEIR), traffic operations at the unsignalized intersection of Keefer Road/State Route 99 intersection will operate at unacceptable levels, during the morning peak period only, for left turn movements from Keefer Road onto State Route 99.

The Specific Plan, through conditions of approval, has been modified to require the County to work with CalTrans on the installation of traffic signals at SR 99 and the new arterial and Keefer Road. The Specific Plan utilizes the existing creeks and sloughs as bicycle and pedestrian trails connecting to the various land use area. This will assist in minimizing the use of the automobile. In addition, the Specific Plan has planned office, commercial and industrial areas which will make local jobs available for persons residing within the North Chico Specific Plan Area and thereby reduce commuting traffic into and out of the area.

The Chico General Plan identifies this land as a future growth area and constrains growth in other areas. The Chico General Plan Map also depicts the Alternative Land Use Plan identified in the FEIR. Growth in the Chico area is further constrained through agricultural preservation policies, zoning, and "the greenline". The Chico General Plan requires the infilling and increased densities to create a more compact urban form. Thus, development within the North Chico Specific Plan (NCSP) area is necessary to accommodate future growth.

2. Air Quality Impacts: As discussed on Pages 10-1 through 10-11 of the Draft EIR, the project will contribute indirect emissions associated with project-related automobile use, and will cumulatively exceed emissions thresholds contained in the Air Quality Attainment Plan.

Implementation of the Specific Plan will contribute indirect emissions associated with project related automobile use, and will cumulatively exceed emission thresholds contained in the Air Quality Attainment Plan. The NCSP, more than any plan in the vicinity is designed to reduce the dependency upon the automobile and to reduce automobile trips. The location of the Village Core, the connection of the Village Core to the various land uses within the Plan, and the extensive trail and pathway system, will contribute to lower emissions than standard urban/suburban development.

Air quality impacts affect a regional area much larger than just the North Chico Specific Plan. Air quality impacts from any development within the Northern Sacramento Valley Air Basin are of a type which would be expected to occur in connection with development anywhere in the

County and can only effectively be addressed on a region-wide basis. It is questionable whether such cumulative impacts attributable to the adoption of this Plan are significant and they could not be completely mitigated without prohibiting all new development. The cumulative impacts cannot be resolved with project specific mitigation measures, but could only be addressed in connection with overall development policies in the Northern Sacramento Valley Air Basin as a whole.

3. Land Use Impacts: As discussed on Page 2-3 of the FEIR, the implementation of the Specific Plan will cause the conversion of approximately 1,630 acres of existing open space to urban/suburban uses. This area has been designated as a growth area for the City of Chico in their proposed General Plan Update. The use of a specific plan for more detailed planning allows for a workable solution to the traffic and drainage problems which have plagued the area.

The Chico General Plan identifies this land as a future growth area and constrains growth in other areas. The Chico General Plan Map also depicts the Alternative Land Use Plan identified in the FEIR. Growth in the Chico area is further constrained through agricultural preservation policies, zoning, and "the greenline". The Chico General Plan requires the infilling and increased densities to create a more compact urban form. Thus, development within the NCSP area is necessary to accommodate future growth.

- F. Project Alternative: In Chapter 5 of the FEIR, the Alternative Land Use Plan to the project represents an environmentally superior alternative to the proposed Land Use Plan contained in the Specific Plan as Figure 3-2. The Alternative Land Use Plan locates proposed residential at densities greater than 1 dwelling unit per acre, to a point 3,700 feet or more from the centerline of the main runway of the Chico Municipal Airport. The Alternative Land Use Plan relocates the main arterial of the proposed Plan out of known wetland habitat, thus minimizing potential impacts to wetlands.

Section 2: Overriding Findings regarding the Airport Land Use Commission. On February 9, 1994 the Airport Land Use Commission found the North Chico Specific Plan inconsistent with the currently adopted Airport Land Use Plan for the Chico Municipal Airport, but did not make findings specifying the reasons for such inconsistency. Government Code Section 65302.3 requires the NCSP to be consistent with the said Airport Land Use Plan, unless the Board of Supervisors does not concur with any provision of the Plan and adopts findings pursuant to Section 21676 of the Public Utilities Code. The Board of Supervisors does not concur with provisions in the Chico Airport Land Use Plan which precludes any development in the airport environs other than 1 acre or larger residential lots and agricultural uses. Furthermore, following the action of the Airport Land Use Commission, an Alternate Land Use Plan

was prepared and is incorporated into the FEIR and the NCSP to provide for the orderly development, expansion, and long term viability of the Chico Municipal Airport and orderly development of the area surrounding the airport. Therefore, the Board of Supervisors makes the following findings pursuant to Public Utilities Code Section 21676.

A. The NCSP provides for the orderly development, expansion, and long term viability of the Chico Municipal Airport and orderly development of the area surrounding the airport as follows:

1. Existing general plan and zoning provisions would have allowed 1 acre parcels north of the airport which could create significant operational concerns for the airport. See attached City of Chico letter dated February 1, 1995.
2. The NCSP reduces existing allowed densities off the north end of the Clear Zone by increasing minimum parcel sizes from 1 acre to 3 acres, as shown on the Alternative Land Use Plan and thus makes the potential development in the area more compatible with the orderly development and expansion and long term viability of the airport;
3. Residential land uses, at densities greater than 1 unit per acre, are not allowed within 3,700 feet from the centerline of the main runway as shown on the Alternative Land Use Map in the FEIR, thus limiting the potential for complaints of incompatibility with airport uses;
4. Pursuant to the Alternative Land Use Plan noise sensitive land uses are buffered from ground generated noise at the airport, such as engine run-up, by locating industrial, commercial and office land uses rather than residential uses, closest to and adjacent to the airport;
- * 5. Open space has been planned for the area westerly of the Clear Zone, northerly of Mud Creek as shown on the Alternative Land Use Plan in the FEIR, further reducing the potential for complaints of incompatibility with airport uses;
- * 6. One acre minimum parcel sizes are planned for the area north and west of Mud Creek as shown on the Alternative Land Use Plan in the FEIR, which is consistent with the currently adopted Airport Land Use Plan for the Airport and with the *Airport Land Use Handbook* pages 3-13, 9-19 through 9-25;

7. Airport expansion through the year 2010 is accommodated by excluding new development within the 55 dB CNEL projected for the year 2010 as projected by the Noise Exposure Map for the Alternative Land Use Plan in Chapter 5 of the FEIR;
8. The Specific Plan provides for airport protection measures as part of the Development Regulations in Chapter 7 of the Specific Plan, pages 7-6 and 7-7;

B. The NCSP protects the public health, safety, and welfare by minimizing exposure to excessive noise and safety hazards within areas adjacent to the Chico Municipal Airport as follows:

1. Intensive uses as shown on the Alternative Land Use Map, including the proposed elementary school, are located abeam the runway, where the great majority of the overflying traffic is light single engine aircraft, at reduced throttle settings prior to landing as stated by the County's consultant, Steve Honeycutt, in public hearings at the Planning Commission on November 16, 1994 and at the Board of Supervisors on January 10, 1995. This is also shown on the *Generalized Flight Tracks* exhibit following page 3-12 of the FEIR and is depicted on the exhibit titled *Generalized Flight Tracks Over Chico Urban Area* attached hereto and presented by the County's consultant, Steve Honeycutt at said hearing on January 10, 1995;
2. Intensive uses as shown on the Alternative Land Use Map, including the proposed elementary school are located inside and away from the Heavy Aircraft Pattern 1 flight track, which will minimize overflight and single event noise occurrences from heavy aircraft, as stated by the County's consultant, Steve Honeycutt in public hearings at the Planning Commission on November 16, 1994 and at the Board of Supervisors on January 10, 1995. This is also shown on the *Generalized Flight Tracks* exhibit following page 3-12 of the FEIR and is depicted on the exhibit titled *Generalized Flight Tracks Over Chico Urban Area* attached hereto and presented by the County's consultant, Steve Honeycutt at said hearing on January 10, 1995;
3. The State Division of Aeronautics, in its letter of May 19, 1992, determined that the proposed elementary school site provides the minimum level of safety suitable for a school;

4. Prior to development of an elementary school at the proposed location, which may be ten years or more in the future, there will be additional opportunities to re-evaluate safety and noise considerations through involvement of the Chico Unified School District, the City of Chico, the State Division of Aeronautics, the Airport Land Use Commission, and the County.
5. Avigation easements are required, as indicated on page 7-6 of the NCSP text, for all residential development within the Plan area;
6. The NCSP provides for airport protection measures as part of the Development Regulation in Chapter 7 of the Specific Plan, pages 7-6 and 7-7;
7. *The Accident Sites for Runways of 6,000 Feet or More*, Figure 8F, taken from the 1994 Airport Land Use Handbook and superimposed with the Alternative Land Use Map for the NCSP (see attached Exhibit A), together with testimony pertaining thereto at the Board of Supervisors' public hearing on January 10, 1995 by the County's consultant Steve Honeycutt, indicates that the accident probability is highest within the boundaries of the Chico Municipal Airport Clear Zones and very low in the vicinity of the Village Core, including high density residential and the elementary school site.
8. The attached Exhibit B, *Comparison of Flight Tracks and School Sites*, together with the testimony pertaining thereto at the Board of Supervisors' public hearing on January 10, 1995 by the County's consultant Steve Honeycutt, indicates that aircraft on flight tracks at Chico Municipal Airport are at a sufficient flight distance and altitude so as to not expose the high density residential and the elementary school sites to excessive noise or safety hazards.

C. The NCSP provide land use measures that minimize the public's exposure to excessive noise and safety hazards within areas around the airport as follows:

1. Noise attenuation is required to be incorporated into all new residential construction as indicated in the additional requirements recommended by the Planning Commission and incorporated into the NCSP in Chapter 4 on page 4-7.;
2. Enhanced disclosure measures, alerting potential buyers and renters of the operations of the airport, avigation easements, and aircraft operations, will be required by Chapter 7, Section 7.6-3 of the NCSP;

3. The NCSP provides for airport protection measures as part of the Development Regulation in Chapter 7 of the Specific Plan, pages 7-6 and 7-7;
4. Intensive uses as shown on the Alternative Land Use Map, including the proposed elementary school are located inside and away from the Heavy Aircraft Pattern 1 flight track, which will minimize overflight and single event noise occurrences from heavy aircraft, as stated by the County's consultant, Steve Honeycutt in public hearings at the Planning Commission on November 16, 1994 and on January 10, 1995. This is also shown on the *Generalized Flight Tracks* exhibit following page 3-12 of the FEIR and is depicted on the exhibit titled *Generalized Flight Tracks Over Chico Urban Area* attached hereto and presented by the County's consultant, Steve Honeycutt at said hearing on January 10, 1995;
5. The State Division of Aeronautics, in its letter of May 19, 1992, determined that the proposed elementary school site provides the minimum level of safety suitable for a school;
6. Prior to development of an elementary school at the proposed location, which may be ten years or more in the future, there will be additional opportunities to re-evaluate safety and noise considerations through involvement of the Chico Unified School District, the City of Chico, the State Division of Aeronautics, the Airport Land Use Commission, and the County.
7. The NCSP reduces existing allowed densities off the north end of the Clear Zone have been reduced by increasing minimum parcel sizes from 1 acre to 3 acres, as shown on the Alternative Land Use Plan in the FEIR and thus makes the potential development in the area more compatible with the orderly development and expansion and long term viability of the airport;
8. Uses involving high concentrations of people, including such uses as social halls, churches, rest homes, convalescent homes, sanitariums and foster or group homes will not be allowed as a matter of right within areas designated as residential zones, but will only be allowed if a use permit is obtained in each case, subject to findings being made, following a public hearing, that such use would not be detrimental to the health and general welfare of the persons residing or working in the neighborhood or to the general health, welfare and safety. Chapter 7 of the NCSP, pages 7-08 through 7-12, and Butte County Code Section 24-45.10.

Section 3: Action

A. Certification of the EIR: Subject to the findings indicated in Sections 1 and 2 of this resolution, certify the Final Environmental Impact Report with mitigation measures as contained in documents entitled *Draft Environmental Impact Report on amendment of the General Plan and adoption of the North Chico Specific Plan*, dated December, 1993, *Final*

Environmental Impact Report on amendment of the General Plan and adoption of the North Chico Specific Plan, dated October, 1994.

B. Adoption of the Specific Plan: Adopt the North Chico Specific Plan amended by the *Revised Draft North Chico Specific Plan*, dated Revised October, 1994 with the conditions indicated below:

1. The Alternative Land Use Plan shown attached and labeled Figure 3-2 shall replace Figure 3-2 contained in the North County Specific Plan (NCSP). This shall be the Land Use Plan for the NCSP.
2. The Final NCSP shall be revised to incorporate all of the changes shown in the Revised NCSP, dated October, 1994 and the mitigation measures of the FEIR, where appropriate, as policies or regulations of the NCSP. The Figures and Tables shall be revised to reflect the Alternative Land Use Plan. All Figures and Tables shall indicate the source, the preparer, and the date of preparation. All references to the Rezone and the General Plan Amendment shall be purged from NCSP, except for a historical reference to this approval process.
3. An Agricultural Policies subsection shall be added to the Chapter 3 - Land Use and a policy shall be added which states as follows:

All existing orchards shall be permitted to continue, without interference or interruption by development.

4. Staff shall work with PG&E to select a street lighting fixture which shall be utilized as the standard street light in the area south of Mud Creek. The Specific Plan shall be amended to require the installation of street lights in the area south of Mud Creek as a requirement of development. In addition, the CSA or other funding mechanism shall be utilized for the payment and maintenance of street lights.
5. The NCSP, Chapter 4 - Circulation, page 4-7, shall be revised to add the following policies:
 8. Enhanced street entries shall be required at the following locations:
 - a. The new arterial/Highway 99

- b. The north and south entry to the industrial area
- c. The new Hicks Road/Eaton Road
- d. The new arterial near Mud Creek
- e. The new arterial near Sycamore Creek

The entry shall consist of landscaping and hardscape. Rock or block walls shall be installed behind the sidewalk. The name of each land use area shall be designed into the Rock or block wall, such as Village Area, Industrial Park, and the like. Shrubbery and at least four specimen size trees shall be used to further enhance the entry. Each entry area shall also have a raised landscaped median with a left turn pocket. The design of the street entries shall be approved by the Directors of Development Services and Public Works.

- 9. A detailed alignment study for Old Hicks Road/New Hicks Road and Eaton Road shall be prepared by or under the direction of Butte County. The study shall indicate the precise engineering of the (right of way) alignment, the removal or relocation of structures, the installation of curb, gutter, sidewalk, and other infrastructure necessary to accomplish the re-alignment.
- 10. Noise attenuation along existing and proposed arterials shall be required to protect residential development proposed to be located adjacent to the proposed arterials. Noise attenuation measures shall be required to reduce interior noise levels to 45 dB for proposed residential development adjacent to existing or planned arterials and/or when adjacent to Highway 99.

- 6. The Alternative Land Use Plan contained in the FEIR indicates a mini park on the new arterial. The Land Use Plan shall be revised to eliminate the mini park.

7. The Alternative Land Use Plan shall be amended to remove the Land Use Summary table, to show the Heavy Industrial area and the surrounding Open Space/Greenbelt as a green and blue cross hatched area and to show an area west of Hicks Lane as R-1 Low Density Residential. In addition, the following note shall be added to the map:

Street alignments are conceptual and subject to precise engineering. The Greenbelt areas along the creeks and sloughs are conceptual in nature. The precise extent of those areas shall be defined on the basis of land division maps, detailed engineered site plans and the like.

8. Add additional policies to the Circulation Chapter 4, which state, "No new street alignments shall exclude safe and convenient access to properties." and "County staff shall pursue negotiations with Caltrans for appropriate traffic control, including but not limited to, traffic signals and street alignments along Highway 99, primarily at the intersections of Keefer Road and the new arterial."
9. All revisions to the NCSP text and maps shall be made within 45 calendar days from the effective date of approval of the related rezone ordinance.
10. For all residential properties of the North Chico Specific Plan, full mitigation of school impacts shall be required in accordance with the following policy added to page 6-13, under Chapter 6, Public Facilities and Services Element, under School Policies:

4. Impacts to school facilities within the Chico Unified School District (CUSD) shall be fully mitigated through the payment of mitigation fees in the amounts justified by CUSD prepared nexus studies, including adjustments, and adopted by the District, or through the implementation of other equivalent measures acceptable to CUSD, including those measures specified in the CUSD Board of Education Resolution No. 486-92.

The County recognizes that the new residential growth contemplated by this Specific Plan will result in significant increases in the student population of the Chico Unified School District ("CUSD"). The County also recognizes that funding limitations have severely hampered CUSD's ability to

accommodate students generated by new residential development. Accordingly, the County, as a condition of approval of this Specific Plan, has required that new residential development fully mitigate its impacts to school facilities. But for this full mitigation requirement, the County would not have approved this Specific Plan.

Section 4 Effective Date

This action shall become effective on April 28, 1995, thirty calendar days from the date of adoption of this Resolution.

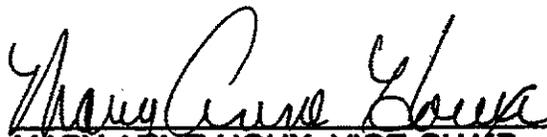
PASSED AND ADOPTED by the Board of Supervisors of the County of Butte, State of California, on the 28th day of March, 1995 by the following vote:

AYES: Supervisors Meyer, Dolan, Thomas and Vice Chair Houx

NOES: None

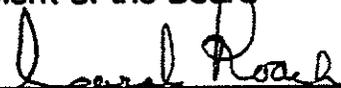
ABSENT: Chair McLaughlin

NOT VOTING: None



MARY ANNE HOUX, VICE CHAIR
Butte County Board of Supervisors

ATTEST: John Blacklock, Chief Administrative Officer
and Clerk of the Board

By  _____

- Attachments:**
- Exhibit A:** Aircraft Accident Characteristics: Accident Sites for Runways of 6,000 Feet or More
 - Exhibit B:** Comparison of Flight Tracks and School Sites
 - Exhibit C:** Generalized Flight Tracks
 - Exhibit D:** Generalized Flight Tracks over Chico Urban Area
 - Exhibit E:** Increases in Residential Densities as a Result of the Specific Plan
- FIGURE 3-2: North Chico Specific Plan Land Use Map**

**Alrcraft Accident Characteristics:
Accident Sites for Runways of
6,000 Feet or More**

Source: Airport Land Use Planning Handbook,
December 1983

Land Use by Heritage Partners and
City of Chico Planning Department for
General Plan Update,
March, 1983

- Arrivals
- Departures

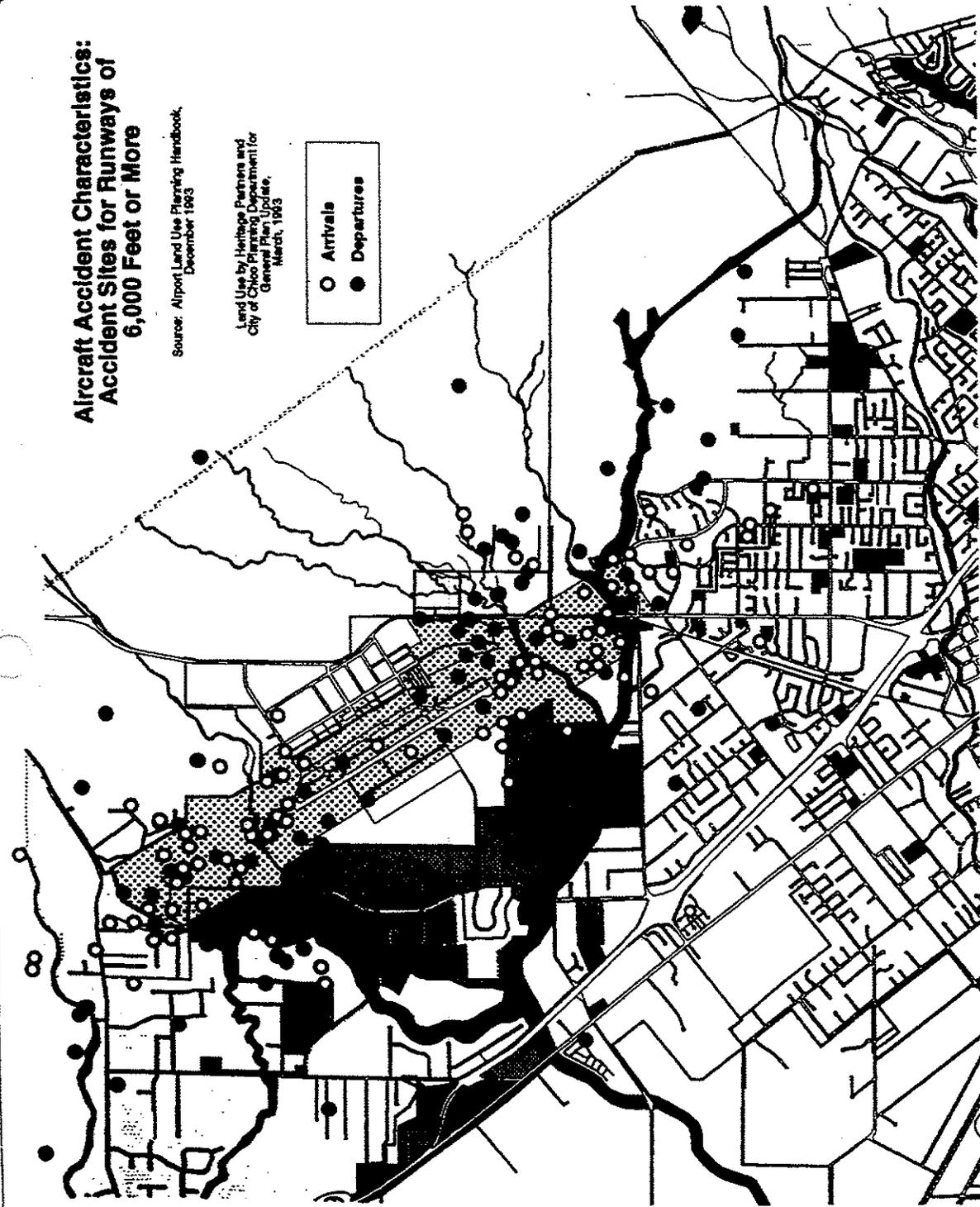
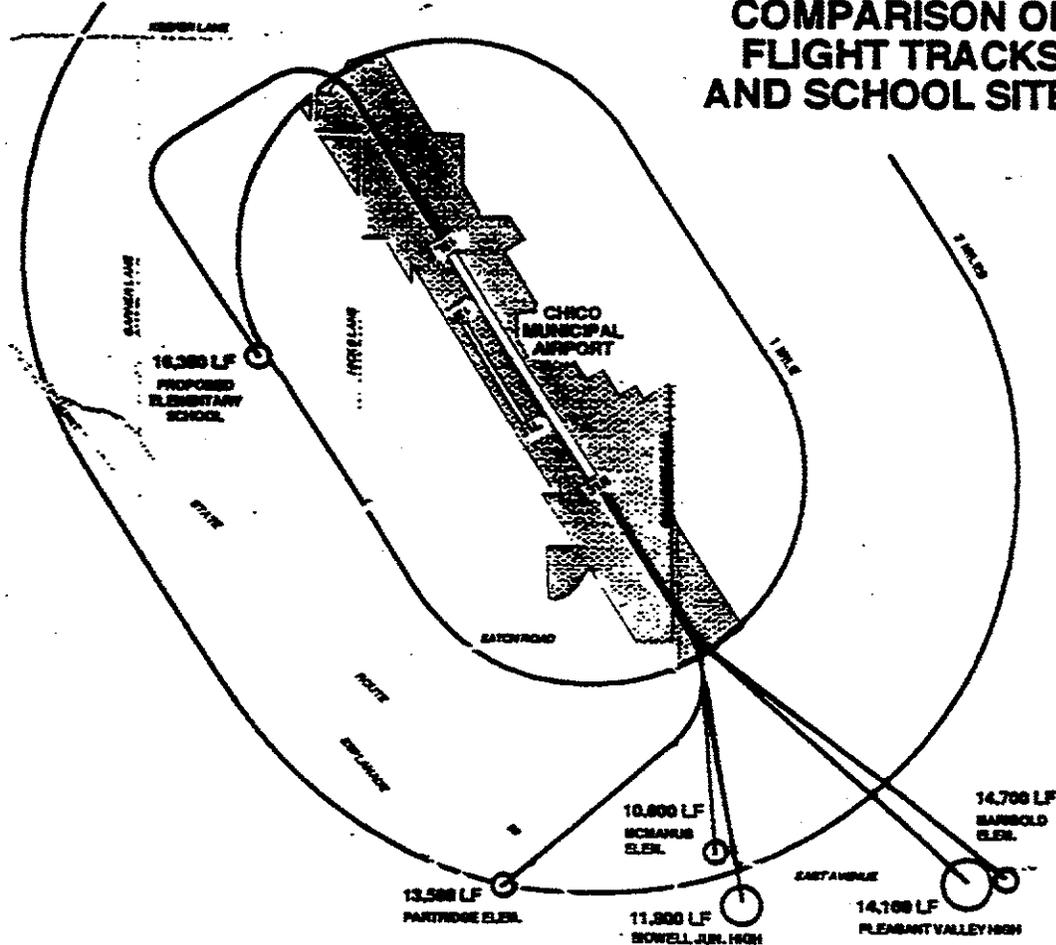


EXHIBIT A

EXHIBIT A

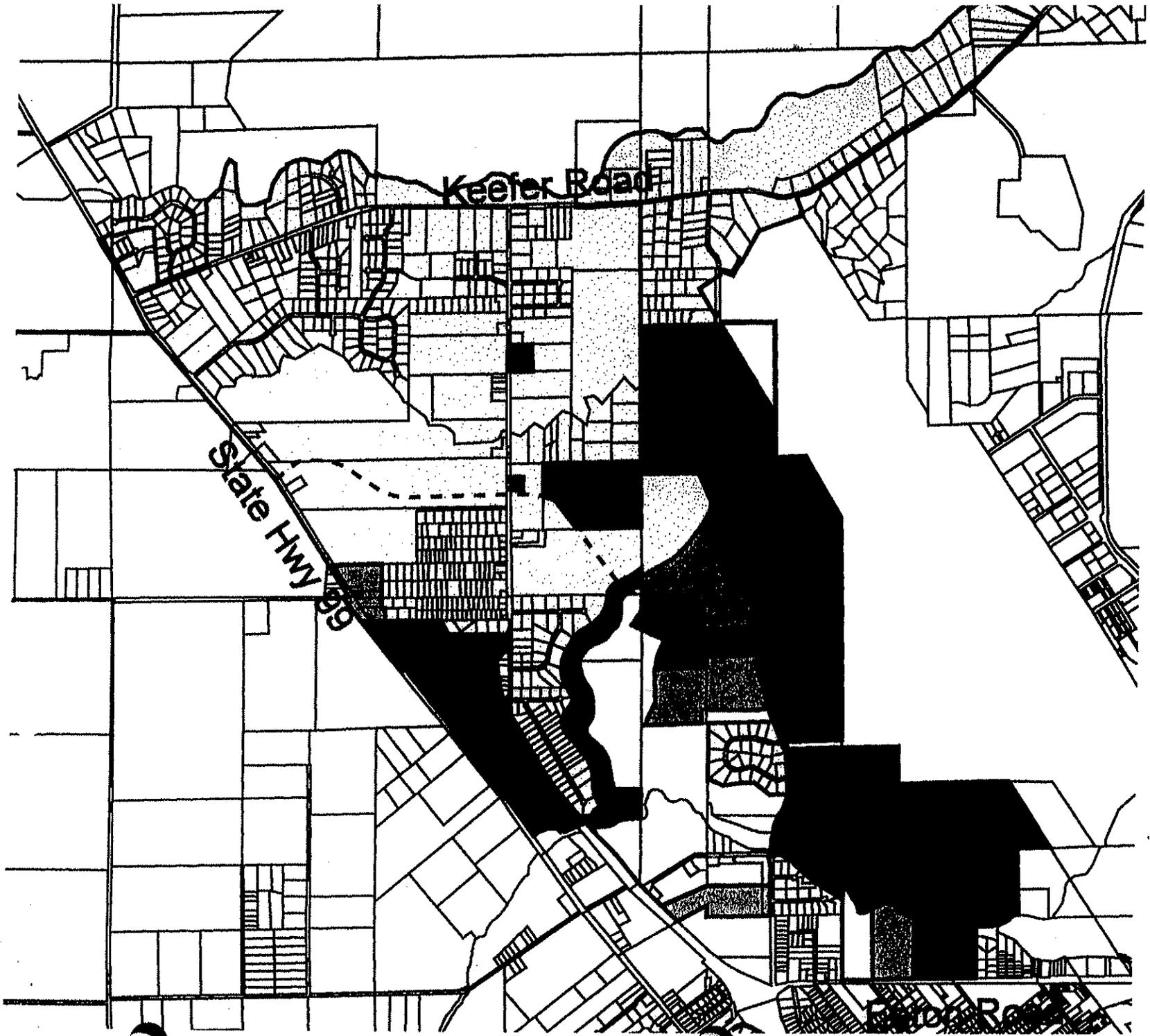
COMPARISON OF FLIGHT TRACKS AND SCHOOL SITES



1. DISTANCE TO RUNWAY ALONG FLIGHT PATH IS THE MOST RELEVANT MEASUREMENT, NOT DIRECT.
2. LOCATION ABEAM THE RUNWAY EQUALS ALTITUDE EQUIVALENT TO 2 1/2 TO 3 MILES
3. PROPOSED LOCATION COMPARES EQUALLY OR FAVORABLY TO BIDWELL JUNIOR HIGH, MCMANUS, AND OTHERS.

EXHIBIT B

Specific Plan



- CSA 87
- - - New Roads
- [White Box] Bk07
- [White Box] Ap06
- [White Box] Ap47
- ZONES**
- [Black Box] B-P
- [Dark Grey Box] C-1
- [Dark Grey Box] C-2
- [Dark Grey Box] M-1
- [Dark Grey Box] M-2
- [Dark Grey Box] OS
- [Dark Grey Box] P-Q
- [White Box] R-1
- [Dark Grey Box] R-2
- [Dark Grey Box] R-3
- [White Box] SR-1
- [White Box] SR-3

ORDINANCE NO.

**AN ORDINANCE OF BUTTE COUNTY AMENDING
THE ZONING FOR A PORTION OF BUTTE COUNTY,
CALIFORNIA, KNOWN AS THE
NORTH CHICO SPECIFIC PLAN AREA**

1 The Board of Supervisors of the County of Butte, State of California, under and
2 pursuant to Chapter 24 of the Butte County Code of said County ORDAINS as
3 follows:

4 SECTION 1. The attached Exhibit A shall be and is hereby adopted as the
5 zoning districts for the North Chico Specific Plan area, and such area shall be subject
6 to the restrictions and regulations of the North Chico Specific Plan text and Chapter
7 24 of the Butte County Code.

8 SECTION 2. This Ordinance shall be and it is hereby declared to be in full
9 force and effect from and after thirty (30) calendar days after the date of its passage,
10 and before the expiration of fifteen (15) calendar days after its passage, this
11 Ordinance shall be published once with the names of the members of the Board of
12 Supervisors voting for and against it in the Enterprise Record, a newspaper published
13 in the County of Butte, State of California.

14 PASSED AND ADOPTED by the Board of Supervisors of the County of Butte,
15 State of California, on the ____ day of _____, 1995, by the following vote:

16 AYES:

17 NOES:

18 ABSENT: